

# Clongriffin Strategic Housing Development 2

An Bórd Pleanála

August 2019

Gerard  
**Gannon**  
PROPERTIES



Architects & Urban Designers Report - Stage 3



Please Note; All drawings within the report are for reference only.  
Refer to full scale drawings for detail.

## PROJECT TEAM

Client	Gerard Gannon Properties
Master Planning	Conroy Crowe Kelly Architects
Planning Consultants	Downey Planning
Architects	Conroy Crowe Kelly Architects Wilson Architecture Downey Planning & Architecture
Landscape Architects	Ronan McDiarmada & Associates
Civil Engineers	Waterman Moylan & Associates
M&E Engineers	DKP International
Environmental	Openfield Ecological Services
Archaeology	Courtney Deery Heritage Consultancy





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# 1.0 INTRODUCTION & BACKGROUND

## 1.1 Introduction

This application is made on behalf of Gerard Gannon Properties. It seeks permission for the construction of 500 apartments and 3,125m<sup>2</sup> of commercial space on lands within Clongriffin Town Centre in Clongriffin, Dublin 13, accessed from Marrsfield Avenue and Main Street, Clongriffin. This application is an extension of the Town Centre and infilling of blocks identified in the original masterplan application (Reg Ref. 0132/02).

This application is one of three applications being lodged concurrently for 1,950 units and 22,727.5m<sup>2</sup> of commercial development across a total of 15 no. infill blocks which formed part of the original masterplan application (Reg Ref. 0132/02). Please refer to the masterplan enclosed with the application for further details.

The proposed housing mix, within this application, comprises 167 no. one-bedroom units, 259 no. two-bedroom units, 25 no. three-bedroom units and 49 no. studio units. 97 of these are proposed for social housing.

The three blocks proposed in this application are called; Block 4, Block 5 and Block 14.

Block 14A is proposed for social housing. Blocks 4 and 14B are proposed for Build to Rent. The remaining Block 5 is proposed Build to Sell. The breakdown of each block is provided on the following pages.

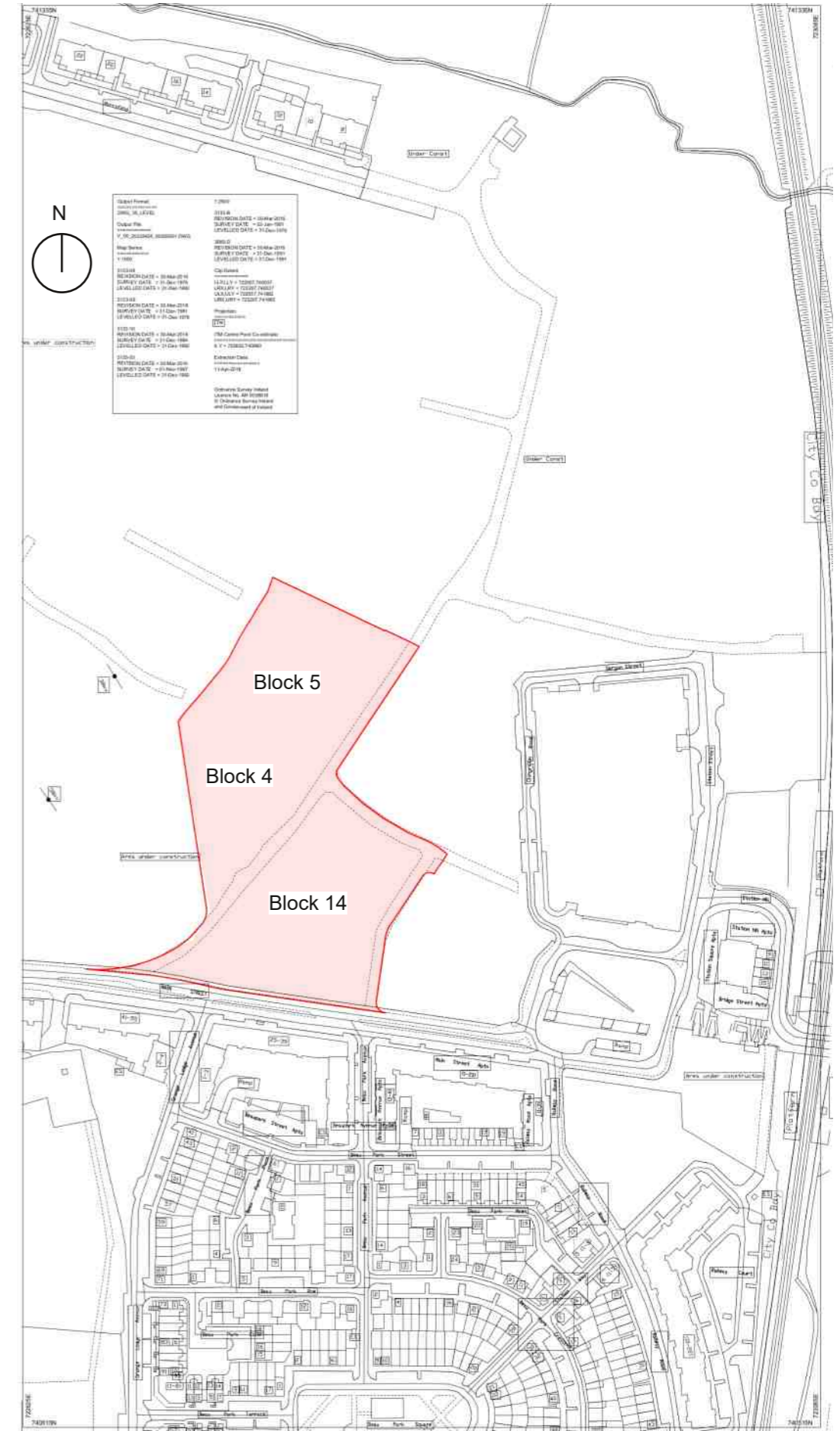
The development includes all associated site works and infrastructure which includes landscaped open space, internal roads, paths, cycle-paths, public lighting, utilities, drainage and surface water attenuation.

This application is one of three concurrent applications. When looked at holistically these three applications will complete the masterplan for Clongriffin Town Centre, within the ownership of Gerard Gannon Properties. Four sites in Clongriffin are outside the applicants ownership and beyond the influence of this application or concurrent applications.

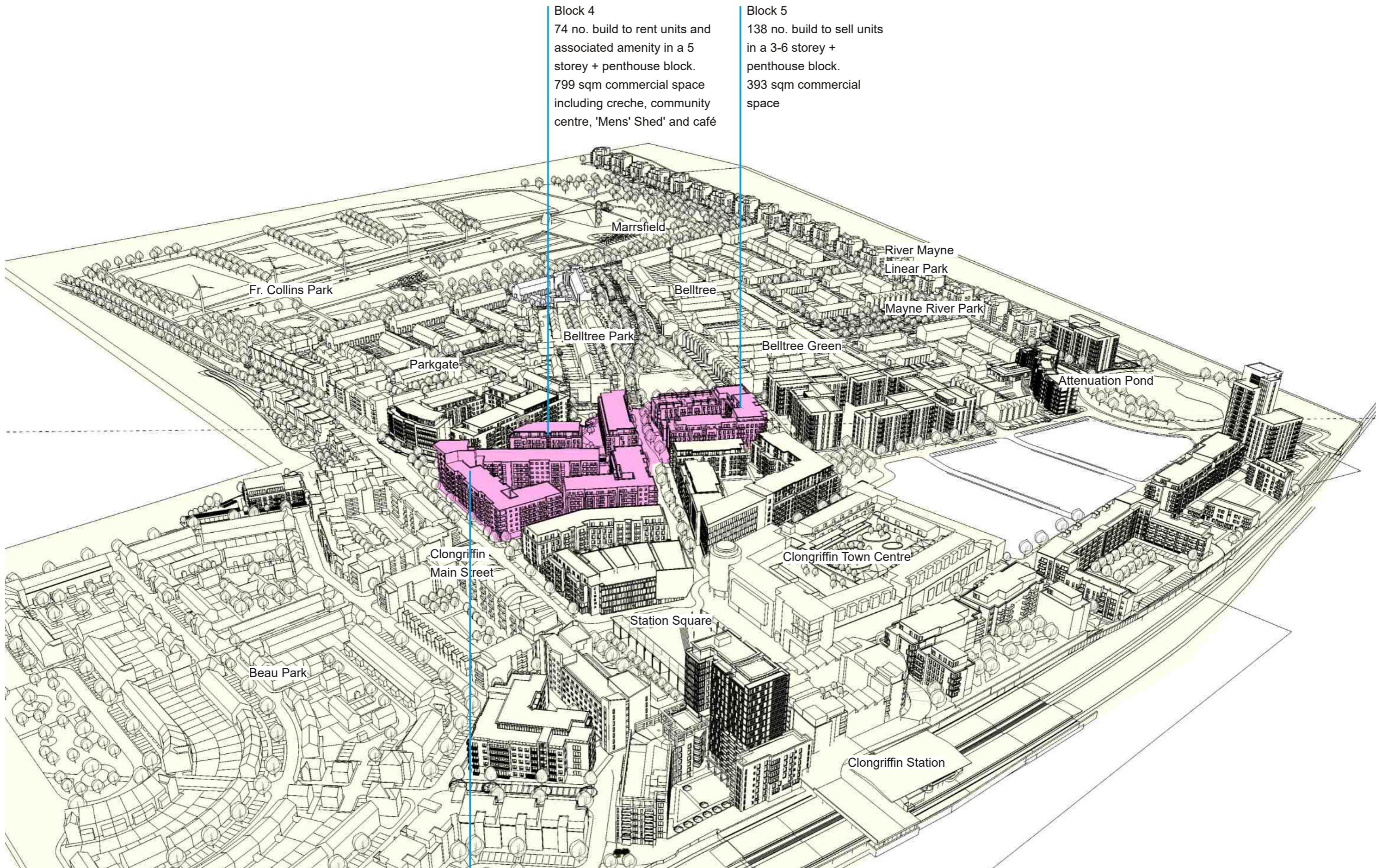
The two concurrent applications are known as 'Clongriffin Strategic Housing Application 1' and 'Clongriffin Planning Application'. Details of these applications can be found later in this report. Briefly, Clongriffin SHD 1 comprises 1,030 no. residential units and 2,285.5m<sup>2</sup> commercial space, Clongriffin Planning Application comprises 420 no. residential units and 17,317m<sup>2</sup> commercial space.

## Key Stats Clongriffin Planning Application

No. of blocks:	3
Total no. of residential units proposed:	500
Density proposed (dwellings per hectare):	200dph
Plot ratio of SHD 1:	2.08
Total site coverage:	71.34%
No. of social housing units:	97 (19.4%)
No. of build to rent units:	265 (53%)
No. of build to sell units:	138 (27.6%)
Total floor area proposed:	51,840m <sup>2</sup>
Quantum of build to rent amenity uses:	1,093.5m <sup>2</sup>
Quantum of town centre commercial:	3,125m <sup>2</sup>
Quantum of public open space:	0m <sup>2</sup>
Area of land subject to this application:	2.49ha
Car parking spaces:	357
Bicycle parking spaces:	1,007
Proportion of parent masterplan completed already (2018):	1/3
Proportion of Gannon owned lands developed/ under construction/ due to be developed (2018):	85%







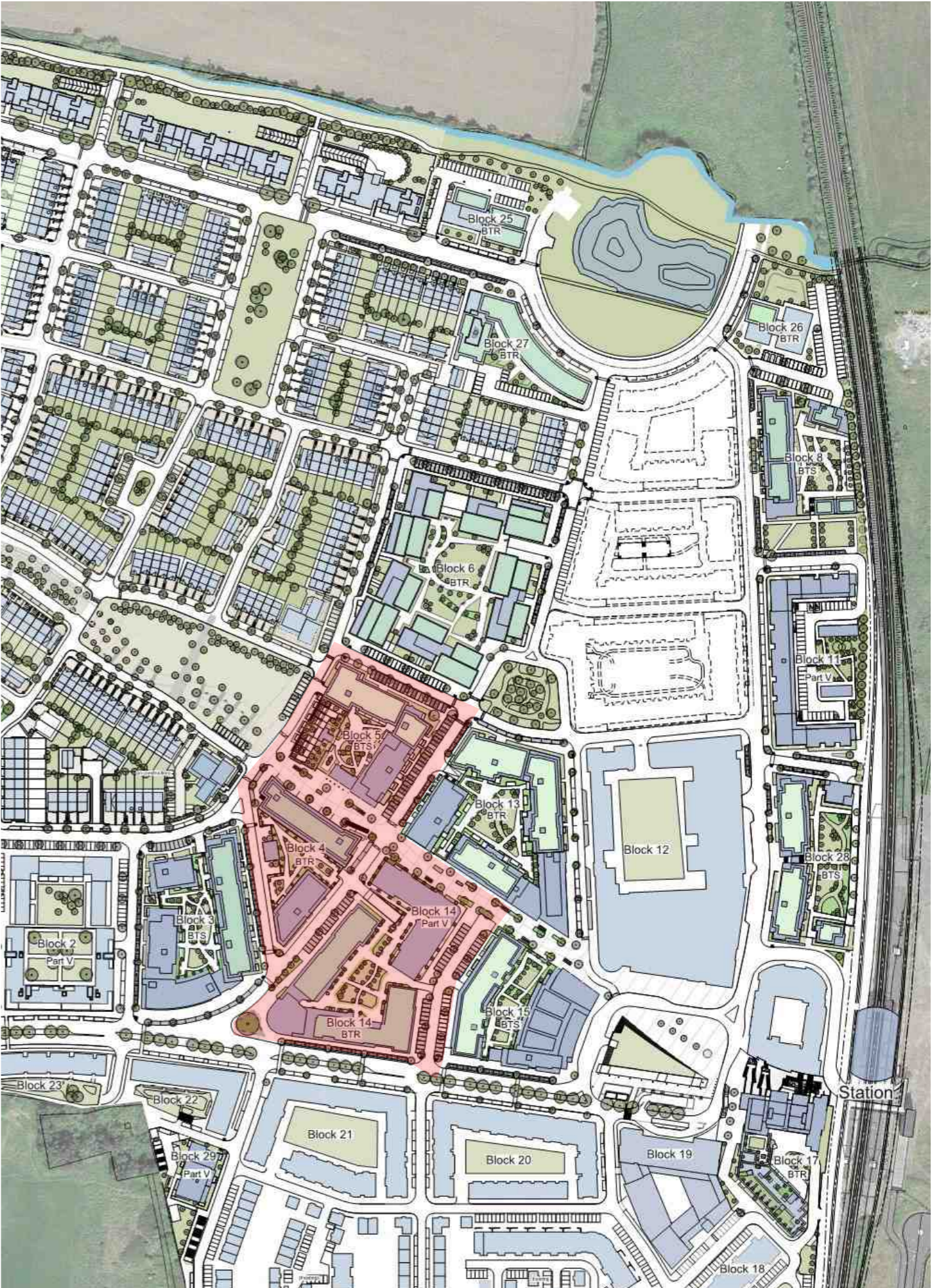
**Block 4**  
 74 no. build to rent units and associated amenity in a 5 storey + penthouse block.  
 799 sqm commercial space including creche, community centre, 'Mens' Shed' and café

**Block 5**  
 138 no. build to sell units in a 3-6 storey + penthouse block.  
 393 sqm commercial space

**Block 14**  
 288 no. total units in two blocks; 97 no. social housing units in Block 14A and 191 no. build to rent units and associated amenity in Block 14B  
 1,127 sqm commercial space



# 1.2 Overview of Proposals



Masterplan site layout showing subject sites - Blocks 4, 5 and 14



## Block 4 - Overview



### Site Summary: Build to Rent

Total Residential Units	74 nr. (277 bed spaces)
Total Commercial Area	799sqm (gross)
Site Area	0.513 ha (1.26 acres)
Plot Ratio	2.03
Site Coverage	95%
Net Density	144 units/ha (59 units/acre)
Building Height	5 storeys + penthouse (residential) 2 storeys (creche)
External Amenity Space (Podium)	1,002 sqm
Internal Amenity Space (BTR)	204.5 sqm
Parking	45 spaces off-street, under podium 5 spaces on-street 50 Total
Cycle Parking	132 spaces

### Schedule of Accommodation

1 Bed Units	9	(avg. 56sqm each)
2 Bed Units	55	(avg. 88sqm each)
3 Bed Units	10	(avg. 105sqm each)
<b>Total</b>	<b>74</b>	
Cafe/ Restaurant	78 sqm (gross)	
Creche	304 sqm	
Community (incl. Mens' Shed)	417 sqm (gross)	
<b>Total</b>	<b>799 sqm (gross)</b>	

## Block 5 - Overview



### Site Summary: Build to sell

Total Residential Units	138 nr. (397 bed spaces)
Total Commercial Area	393sqm (gross)
Site Area	0.70 ha (1.72 acres)
Plot Ratio	2.13
Site Coverage	100%
Net Density	197 units/ha (80 units/acre)
Building Height	3 to 6 storeys + penthouse
External Amenity Space	1,561 sqm (including roof terrace)
Parking	54 spaces off-street, under podium 42 spaces on-street 96 Total
Cycle Parking	224 spaces

### Schedule of Accommodation

1 Bed Units	52	(55.1sqm each)
2 Bed Units	83	(86.3sqm each)
3 Bed Units	3	(117.4sqm each)
<b>Total</b>	<b>138</b>	
Retail	393 sqm (gross)	
<b>Total</b>	<b>393 sqm (gross)</b>	

## Block 14 - Overview



### Site Summary: Social Housing and Build to Rent

Total Residential Units	288 nr. (861 bedspaces) (97 Part V, 191 BTR)
Total Commercial Area	1,933sqm (gross)
Site Area	0.97 ha (2.4 acres)
Plot Ratio	2.7
Site Coverage	47%
Net Density	296 units/ha (121 units/acre)
Building Height	6 to 8 storeys
External Amenity Space	1,977 sqm
Internal Amenity Space	745 sqm
Parking	162 spaces off-street, under podium 49 spaces on-street 211 Total
Cycle Parking	651 spaces

### Schedule of Accommodation

Studios	49
1 Bed Units	106
2 Bed Units	121
3 Bed Units	12
<b>Total</b>	<b>288</b>
Commercial/ Retail	1,127 sqm (gross)
Café/ Restaurant	806 sqm (gross)
<b>Total</b>	<b>1,933sqm (gross)</b>



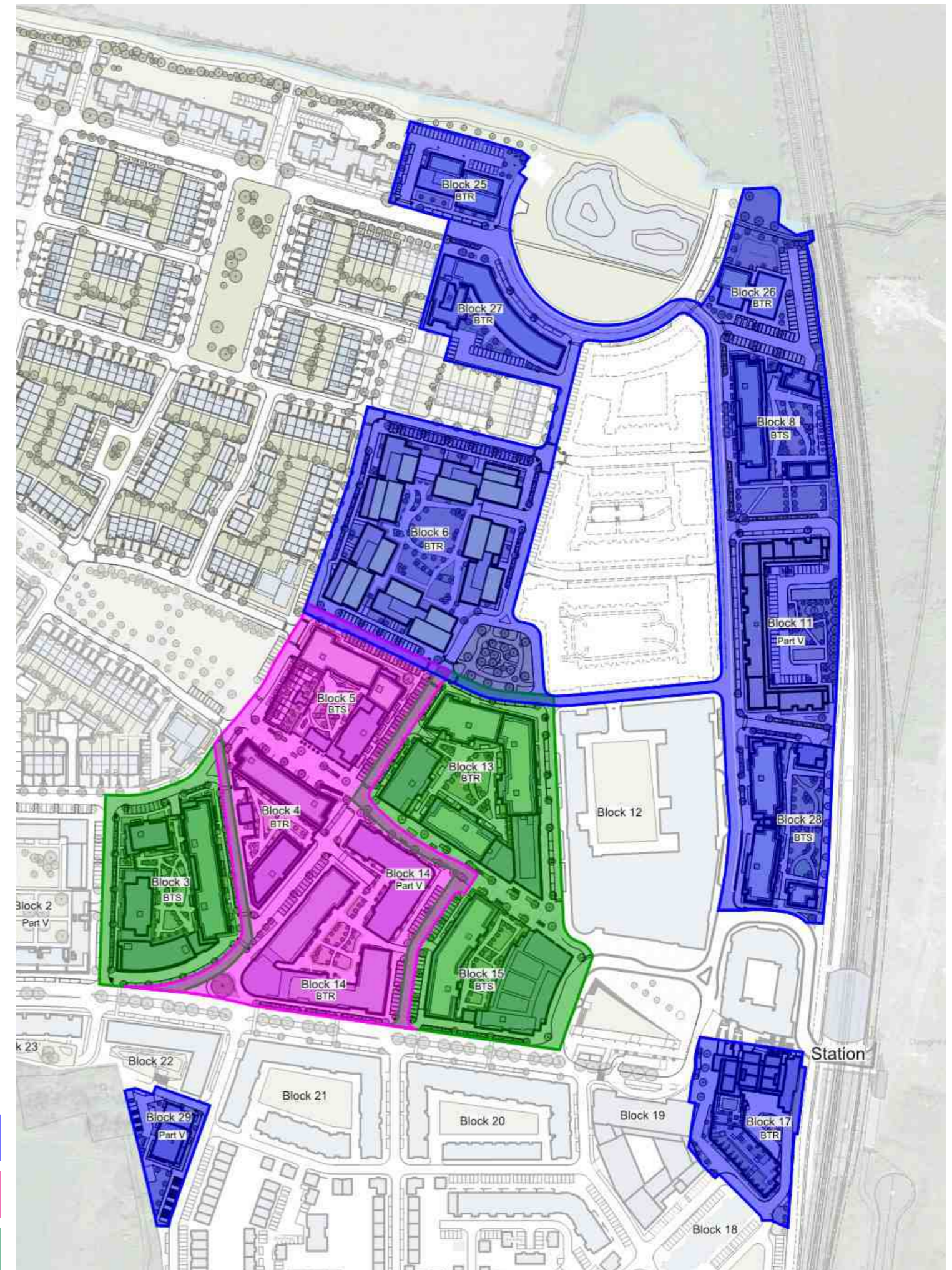
# 1.3 Concurrent Applications

This application is part of a suite of three concurrent applications; two SHD applications to An Bórd Pleanála and one Planning Application to Dublin City Council. Each application is self-sufficient in terms of infrastructure, mix and buildability. When taken together, the three concurrent applications represent the completion of Clongriffin Town Centre.

This application should be viewed in light of the entire Town Centre, including built phases, those phases currently under construction, and future phases. It is the combination of earlier, medium density, phases, focussed on single family homes and public open space, in combination with the subject and concurrent applications, focussed on high density town centre uses, that will combine to complete the vibrant and diverse new town centre sought in Clongriffin.

This report makes reference to these concurrent applications, particularly when discussing macro issues such as permeability, density, mix, social housing, community uses etc. Care has been taken to demonstrate the unique contribution of the subject application, whilst being mindful of the emerging context.

All three applications adhere to the principals set out in the Clongriffin Masterplan (July 2018) and the parent permission (Reg Ref. 0132/02). No deviation is sought from the block layout, roads, open space provision or social housing provision. As such, this subject application, Clongriffin SHD 2, fits into an agreed context in terms of urban structure, density and mix of uses.



Clongriffin SHD 1 ■

Clongriffin SHD 2 ■

Clongriffin Planning Application ■



- Clongriffin SHD 1
- Clongriffin SHD 2
- Clongriffin Planning Application



Bird's Eye View of Clongriffin SHD1, Clongriffin SHD 2 and Clongriffin Planning Application boundaries



## 2.0 SITE CONTEXT & PLANNING

### 2.1 Site Description

Clongriffin is a new town on Dublin's North fringe. The town is designated as Key District Centre 1 in the Dublin City Development Plan. The lands are located within the functional area of Dublin City Council along its northern fringe, with Fingal County Council having jurisdiction over the lands to the north and east.

For a new development area, Clongriffin is relatively close to Dublin city centre, only some 9.4km distance. It is served by Clongriffin Dart Station which is located at the heart of the subject site and will allow new residents frequent access to the city centre and beyond. The bus service from the town uses the Malahide QBC into Dublin city. The lands also connect into the cycle network linking to Dublin city and along the coast.

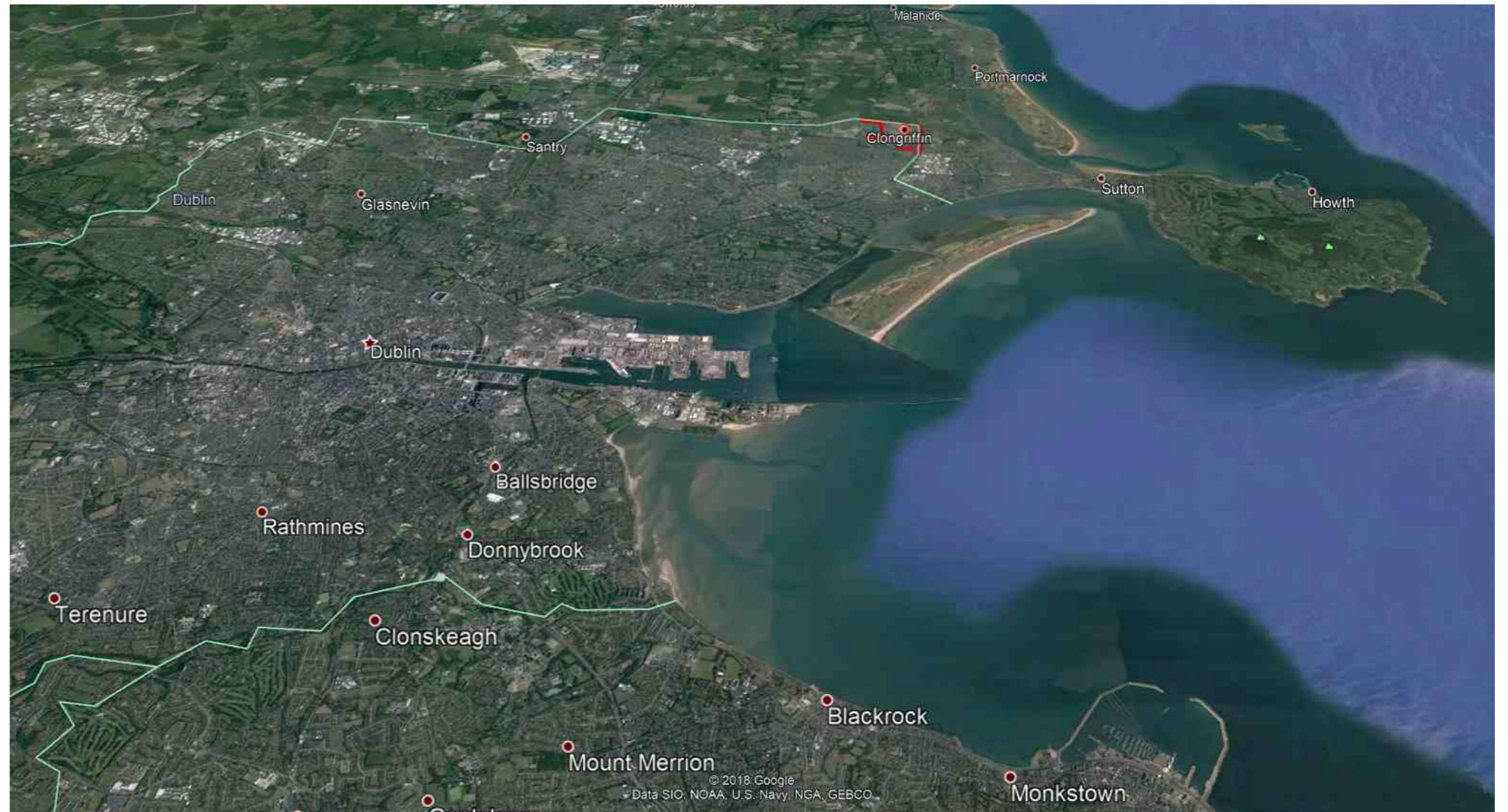
The primary foul and surface water drainage network was constructed under the Clongriffin masterplan grant (Reg. Ref. 0132/02). The network has been designed and constructed to accommodate the subject blocks (including those in concurrent applications), with capped ends provided to enable future connection. A detailed report on site services can be found in the engineer's report by Waterman Moylan and associated drawings submitted as part of this application.

Below is a summary of all the works completed to date, which are directly adjacent to and vital for the prosperity of the proposed development:

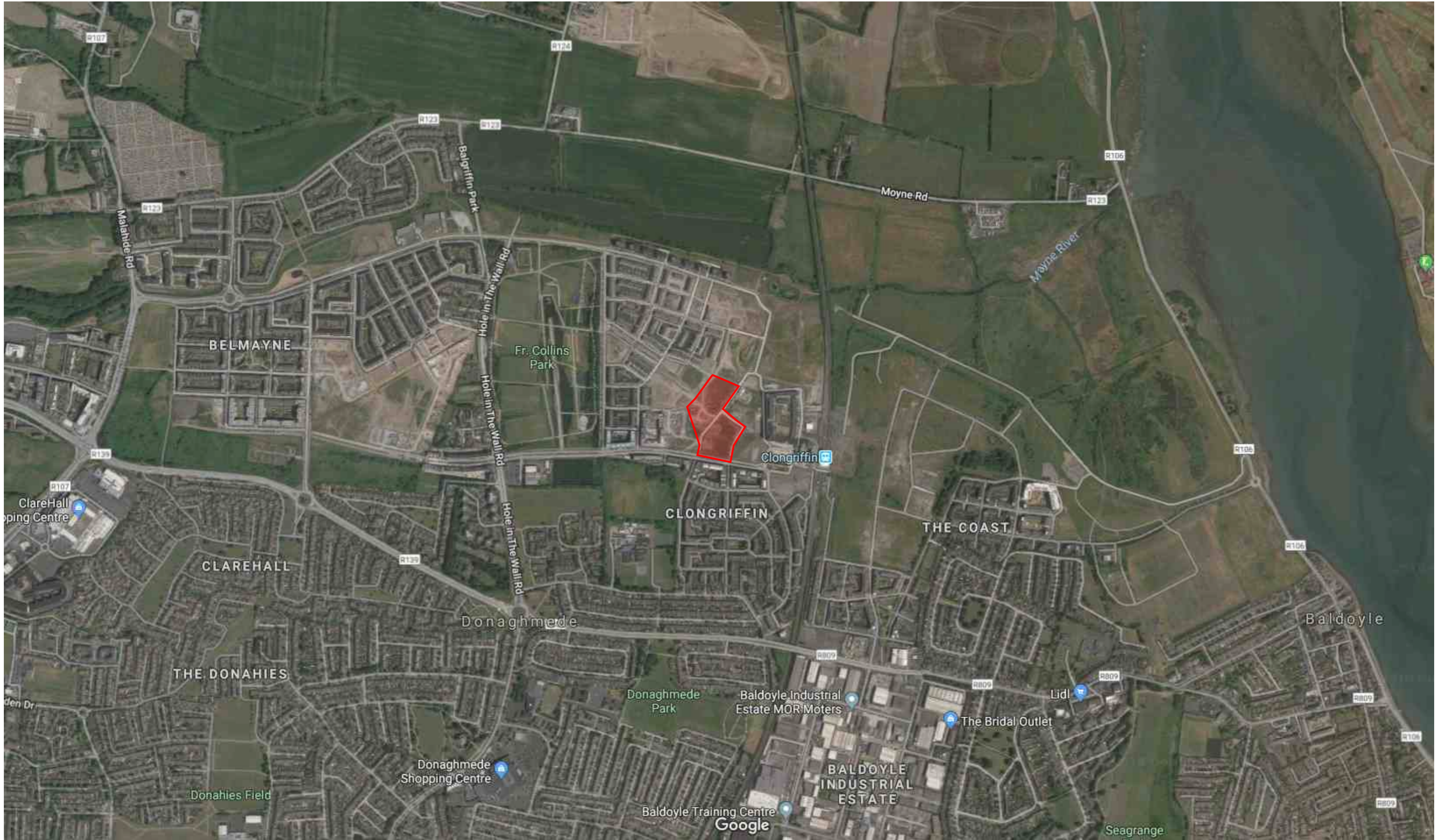
- Clongriffin Main Street, including the extension of the Malahide Road QBC up to the DART station;
- Landscaping of principal streets and public spaces, including the main town square (Station Square) and high-quality civic steps to the train station;
- Re-development and enhancement of Fr. Collins Park (with assistance from special contributions from the completed development);
- Mayne River linear park – 350m completed up to the new attenuation pond and Linear Park between Fr. Collins Park and Station Square under construction;
- A 483 space public Park and Ride facility beside the railway station and the QBC;
- A 600+ public multi-storey car park to serve the new town;
- Construction of the entire internal road network up to wearing course;
- Construction of the entire internal drainage network (foul and surface water) with ongoing upgrades for SuDS as alterations are made to the permitted development by way of new planning applications;
- Completion and operation of the new DART station – this was entirely funded by Gannon Homes Ltd and Ballymore Homes (Helsingor), adjacent landowners in Fingal County Council.
- In January 2019 2,270 units were complete, under construction or permitted.
- 13,950sqm commercial and retail development has been completed.



Station Square looking from steps to Dart Station







Aerial photo of Clongriffin showing site outline for Clongriffin SHD2



## 2.2 Planning History

The proposed site is situated in the centre of Clongriffin which is identified as a 'Key District Centre' (KDC1) in the Dublin City Development Plan 2016-2022.

The lands are identified as a 'Strategic Development and Regeneration Area 1 (SDRA1)' in the current Dublin City Development Plan (2016-2022), which also sets out 3 guiding principles in S.15.1.

The site is governed by the Clongriffin-Belmayne Local Area Plan 2012-2018 which sets out the guiding principles for new development. This Local Area Plan (LAP) adopted in December 2012 has been the blueprint for development in Clongriffin and Belmayne since then. In November 2017 DCC resolved to extend the term of the LAP until 2022. The proposed development aims to meet and exceed the LAP objectives set out by DCC and an overview of this can be found in Chapter 2.

Gerard Gannon Properties sought a 10-year planning permission for the development of a new mixed-use development with town centre facilities on a 133 acre site north of Grange Road, Donaghmede, Dublin 13 in January 2002. This was an ambitious plan and a single application was made for the entire development which originally comprised some 3,600 dwellings and 80,500sqm of commercial development. Subsequent amendment permissions increased the total commercial offer to 100,000sqm.

On the 27th June 2003, An Bord Pleanála granted full planning permission for the development, subject to 46 conditions.

Since the parent planning permission was granted in June 2003, more than 70 separate planning applications have been submitted to the Local Authority for various alterations and revisions, which range in size from small changes-of-use to the applications for large blocks of urban housing.

The 10 year parent permission withered in 2013 but, prior to this, Clongriffin had the benefit of cumulative planning permissions for 3,565 dwellings and c.96,625sqm of non-residential uses.

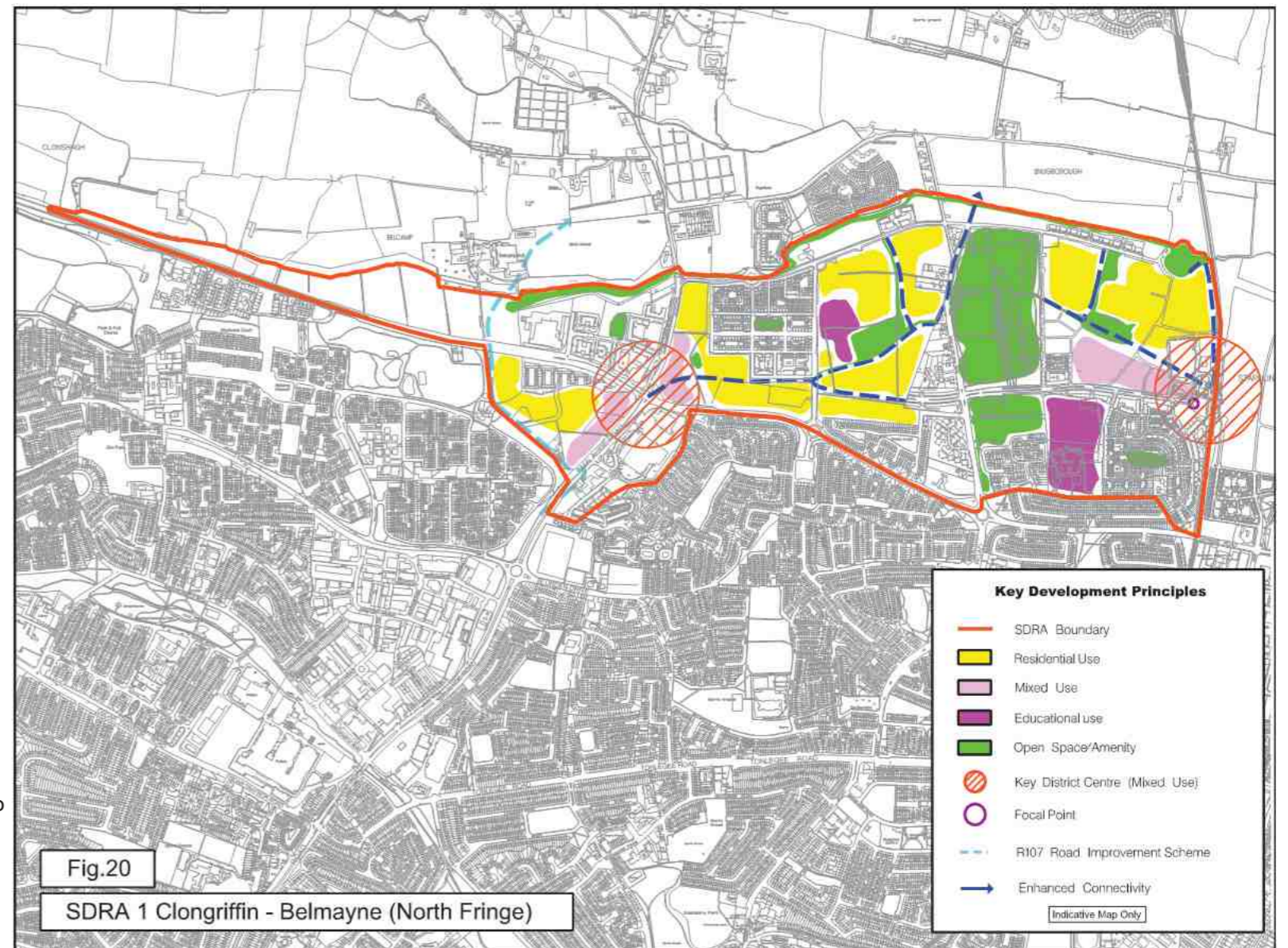
Currently, Gannon Properties have 'live' planning permissions for 345 units, a pre-planning masterplan for c.110 units, and estimated future developments of c.1000 units (subject to detail design).

These figures exclude the Barina and Hollybrook Sites which have an estimated overall capacity of 335 and c.338 respectively.

As of January 2019 1,685 dwellings and c.13,950sq.m. of commercial uses have been completed within the new town with a further 503 dwellings under construction.

Further information on Planning compliance and detail can be found in the separate Planners Report prepared by Downey Planning.

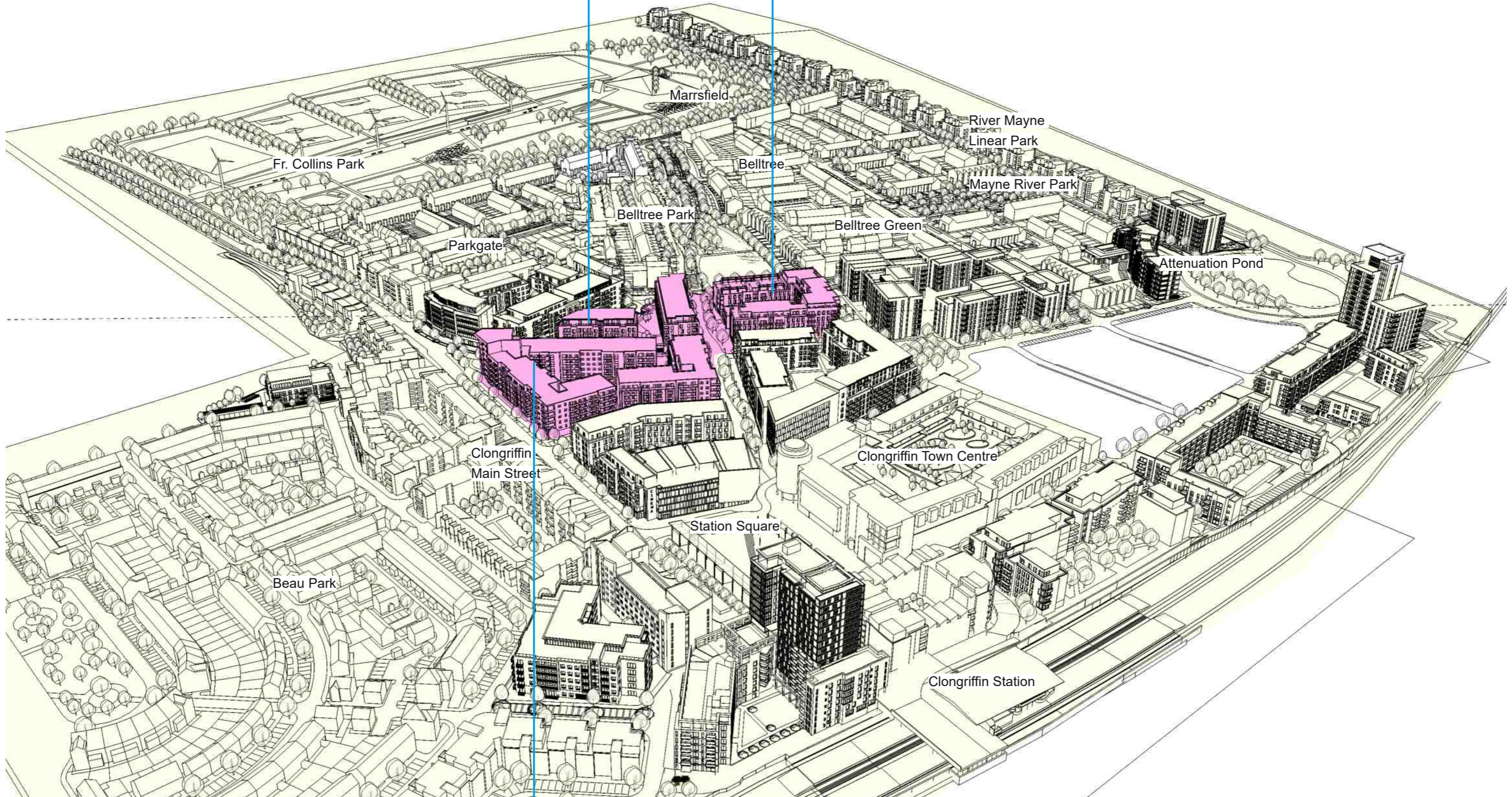
Main Street looking west to Fr. Collin's Park (above), Map SDRA1 from Clongriffin-Belmayne LAP (below)





Block 4  
Reg Ref. 0132/02  
(Parent permission).  
Permission withered in  
2013.  
New permission sought.

Block 5  
Reg Ref. 0132/02  
(Parent permission).  
Permission withered in  
2013.  
New permission sought.



Planning History for all blocks in Clongriffin SHD2

Block 14  
Reg Ref. 0132/02  
(Parent permission).  
Permission withered in  
2013.  
New permission sought.



## 2.3 Masterplan to Date

Gerard Gannon Properties own the majority of lands to the east of Fr. Collins Park and have invested a great deal in the town from its inception. Between the granting of the parent permission in 2003 and 2013 construction investment in Clongriffin was estimated at €350,000,000.

In addition to the great number of dwellings completed, and the construction of purpose-built retail and commercial units to shell and core status, there has been enormous investment (financial, planning and design) into the infrastructure of the new town. Approximately 85% of the overall infrastructure (roads, drainage, services and landscaping) are now complete.

Roughly one third of the Planning Permission for a mixed-use new town centre was constructed when the recession intervened and progress slowed considerably. Construction restarted four years ago and approx. 85% of the lands in Gannon ownership have now been developed, are under construction or are due to be developed in 2019. This has contributed to a thriving town and a vibrant local community.

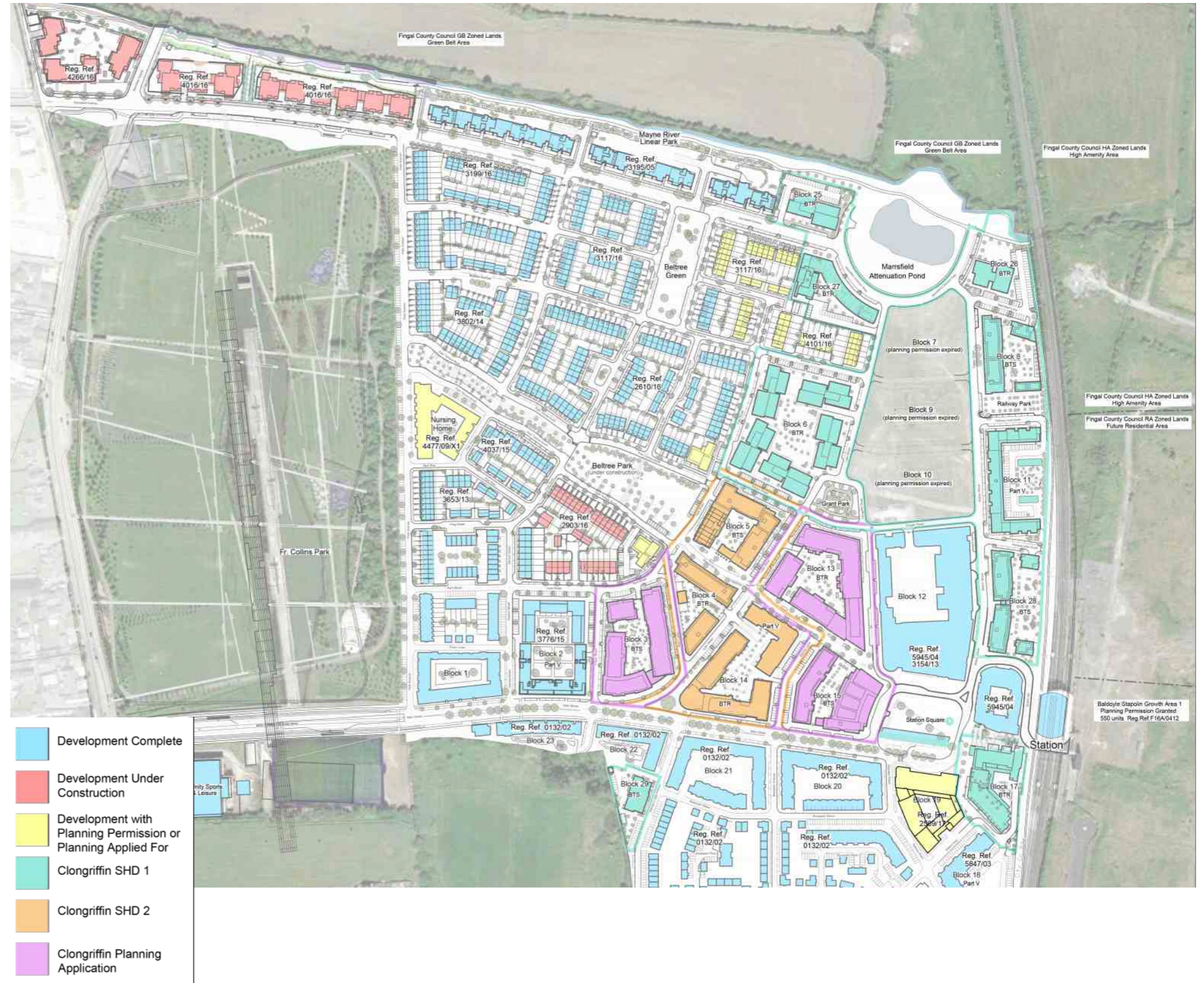
A detailed updated master plan document was prepared by Conroy Crowe Kelly Architects and submitted to Dublin City Council in July 2018. Gannon Homes and Conroy Crowe Kelly have worked with Dublin City Council (then Dublin Corporation) from the late 1990s to firstly establish a design-led masterplan in collaboration with Urban Initiatives Studio. A copy of this Masterplan document is included with this application for reference.

To date 1,685 dwellings and c.13,950sq.m. of commercial uses have been completed within the new town. This includes houses at Beau Park, Park Avenue, Belltree, Park Terrace and Park Edge, in addition to mixed-use blocks along Main Street and around Station Square, including Blocks 1, 12, 16, 20, 21, 22 and 23.

Road infrastructure and utilities have largely been installed and green infrastructure, pedestrian permeability and links to the wider community have all been protected. The overall trend has been to increase density, reduce parking and improve the urban environment for all users where possible.

Several plots are currently on site and will be complete in 2019. These include the recently completed 84 social housing apartments in Block 2, 45 homes under construction in Park Street and the Belltree Park open space (formerly Panhandle Park). Construction is ongoing on houses at Belltree Green.

This application is applying for permission to infill three of the remaining blocks in the client's ownership, all of which have withered planning permissions, with new permissions sought. The proposed blocks, with a primary use of residential above ground floor commercial uses, will provide a sustainable addition to complete this new urban centre.







Park Avenue & Belltree Park



Marrsfield Avenue



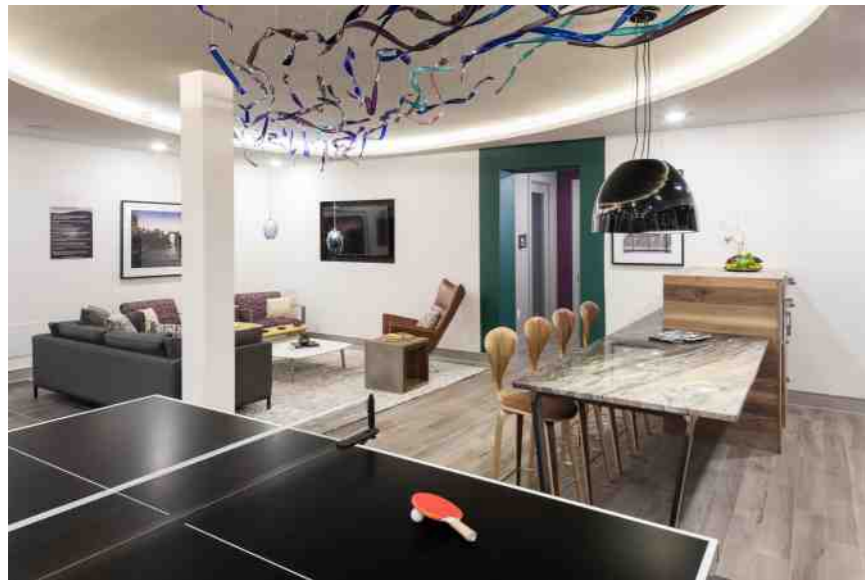
Belltree Avenue



Panorama photo taken June 2018 showing completed and under construction phases of Clongriffin Masterplan



## 2.4 Build To Rent



A large number of blocks from this and concurrent applications are being proposed as Build to Rent. Within this application Block 4 and Block 14B are being built on the basis of a Build to Rent (BTR) model, representing 265 units. Specific details are included in the respective Architects and Urban Designer Reports.

This typology is subject to specific requirements in Sustainable Urban Housing: Design Standards for New Apartments (March 2018) and compliance with such is detailed below. One benefit of BTR is the speed and scale at which projects can be taken to the market. Unlike traditional models, where individual landlords may purchase units and rent them privately, the entire BTR block can be available on the market soon after completion, bringing a large volume of high quality rental units into a market currently suffering severe supply shortages.

The BTR model has been chosen for Clongriffin for several reasons. The primary reason is a general shortage of this type of product in the Dublin rental sector. The 2016 census showed that 30% of households in Dublin are renting. BTR offers many advantages for renters; tenancies are more secure for residents, with longer terms frequently available, purpose-built BTR apartment buildings have dedicated facilities such as laundries, gyms and concierges and buildings will be well managed and maintained over the long term.

There are also less tangible benefits from this model of housing. Research in the field is showing that the provision of communal facilities allows residents opportunities to informally meet their neighbours. Knowing even a small number of neighbours fosters a strong sense of community, which is beneficial for mental health. In addition, BTR offers residents an opportunity to rent through a company with a long term view, removing some of the uncertainty innate in the current rental market, which is predominantly made up of small scale, individual, private landlords.

### Specific Planning Policy Requirement 7:

Section 5 of Sustainable Urban Housing: Design Standards for New Apartments (March 2018) covers detail on the aspirations and also policy around this emerging housing type. Each block in this application complies with these requirements in its own right.

Typical facilities include;

(a) Resident Support Facilities; a shared entrance with concierge, a laundry room and a high quality landscaped podium courtyard. All blocks will be managed by institutional management companies who will take responsibility for maintenance/repair, waste management and any other issues arising.

(b) Resident Services and Amenities; Amenity rooms which will be available for use by residents, typically with a booking system in place, some shared lounges and function rooms. In addition, each of the blocks have additional retail and commercial units at ground level which will add further amenity subject to market demand.

### Specific Planning Policy Requirement 8:

(i) A marketable mix of 27% 1 bed, 50% 2 bed, 6% 3 bed and 17% studios has been applied across the blocks. This reflects the demand for rental units in this location.

(ii) The full storage requirement has been located within apartments as this is most convenient for residents, particularly when a long term view towards tenancies is being taken. All private amenity space is provided at least to the minimum standards, typically more.

(iii) Car parking in each block is provided on an average rate of 0.64 spaces per unit, with overflow and short term parking available in the existing commuter car park in Block 12. This approach has proven successful in earlier phases of development in Clongriffin. Car share vehicles will also be available for communal use. There are currently several car share vehicle available in the Block 12 car park and these have proven extremely successful to date. One additional car share space is proposed adjacent to Block 17, in Clongriffin SHD 1.

(iv) The majority of apartments do exceed the minimum floor area standards by 10%.

(v) No more than 12 apartments are proposed per core.

### Specific Planning Policy Requirement 7

BTR development must be:

- (a) Described in the public notices associated with a planning application specifically as a 'Build-To-Rent' housing development that unambiguously categorises the project (or part of thereof) as a long-term rental housing scheme, to be accompanied by a proposed covenant or legal agreement further to which appropriate planning conditions may be attached to any grant of permission to ensure that the development remains as such. Such conditions include a requirement that the development remains owned and operated by an institutional entity and that this status will continue to apply for a minimum period of not less than 15 years and that similarly no individual residential units are sold or rented separately for that period;
- (b) Accompanied by detailed proposals for supporting communal and recreational amenities to be provided as part of the BTR development. These facilities to be categorised as:
  - (i) **Resident Support Facilities** - comprising of facilities related to the operation of the development for residents such as laundry facilities, concierge and management facilities, maintenance/repair services, waste management facilities, etc.
  - (ii) **Resident Services and Amenities** – comprising of facilities for communal recreational and other activities by residents including sports facilities, shared TV/lounge areas, work/study spaces, function rooms for use as private dining and kitchen facilities, etc.

### Specific Planning Policy Requirement 8

For proposals that qualify as specific BTR development in accordance with SPPR 7:

- (i) No restrictions on dwelling mix and all other requirements of these Guidelines shall apply, unless specified otherwise;
- (ii) Flexibility shall apply in relation to the provision of a proportion of the storage and private amenity space associated with individual units as set out in Appendix 1 and in relation to the provision of all of the communal amenity space as set out in Appendix 1, on the basis of the provision of alternative, compensatory communal support facilities and amenities within the development. This shall be at the discretion of the planning authority. In all cases the obligation will be on the project proposer to demonstrate the overall quality of the facilities provided and that residents will enjoy an enhanced overall standard of amenity;
- (iii) There shall be a default of minimal or significantly reduced car parking provision on the basis of BTR development being more suitable for central locations and/or proximity to public transport services. The requirement for a BTR scheme to have a strong central management regime is intended to contribute to the capacity to establish and operate shared mobility measures;
- (iv) The requirement that the majority of all apartments in a proposed scheme exceed the minimum floor area standards by a minimum of 10% shall not apply to BTR schemes;
- (v) The requirement for a maximum of 12 apartments per floor per core shall not apply to BTR schemes, subject to overall design quality and compliance with building regulations.



# 2.5 Tall Buildings Strategy

In accordance with the LAP objectives building heights have been employed to reinforce street hierarchies, aid way-finding and structure public spaces. Earlier phases delivered houses at two and three storeys. This application proposes three apartment blocks in perimeter block form with five or six storeys plus penthouse above. Two taller buildings are proposed in concurrent applications; Block 17 at Clongriffin Station and Block 26 beside the River Mayne Linear Park.

This strategy locates taller buildings and density in the locations most proximate to public transport including bus stops, cycle lanes and Clongriffin station. The heights proposed will help to enclose streets and, when combined with active frontages through commercial and retail uses, provide a Town Centre feel.

## Clongriffin- Belmayne LAP

The LAP is not overly prescriptive in building heights. However the KDC at Clongriffin Station is identified as an appropriate location for increased density and height. In general minimum heights of five storeys in the KDC and four to five storeys along the Main Street Boulevard should be achieved. The proposals align with this aspiration.

A landmark commercial building adjacent to Clongriffin Station, closing the vista of the Main Street Boulevard and signifying the town centre location to the wider landscape was approved and is still requested. This building will be provided in the form of Block 17, which is subject to a concurrent application. Block 17 as proposed will be 17 storeys in height with mixed use commercial located at the the lower floors and residential above.

The LAP defers to the criteria set out in the Dublin City Development Plan for detail on tall building proposals.

## Dublin City Development Plan (2016-2022)

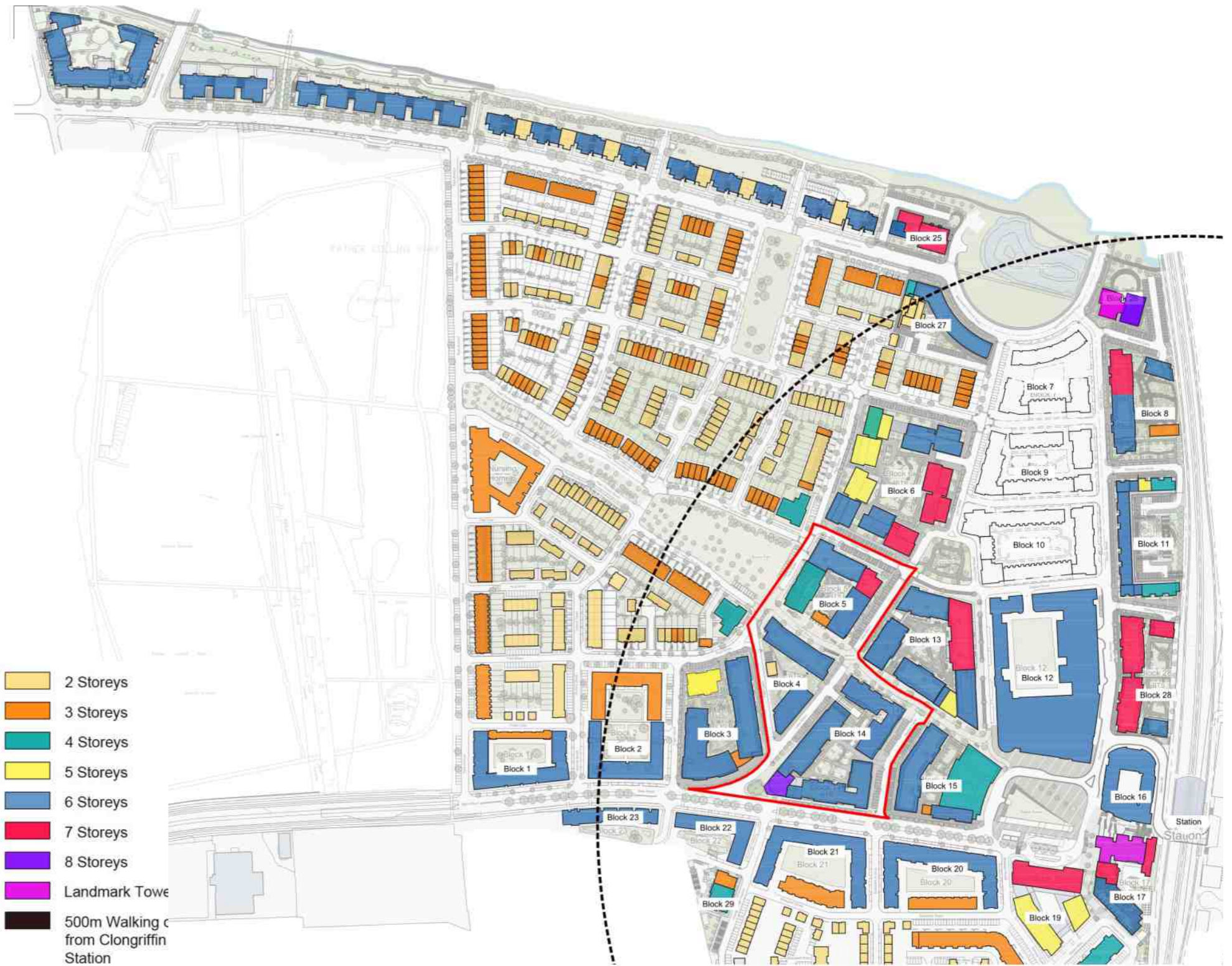
The Development Plan identifies Clongriffin as KDC1 and designates it accordingly as a location for taller buildings and increased density.

## Sustainable Urban Housing: Design Standards for New Apartments (March 2018) & National Planning Framework

These documents encourages the removal of “rigidly applied, blanket planning standards in relation to building design, in favour of performance based standards to ensure well-designed high quality outcomes. In particular, general blanket restrictions on building height or building separation distance that may be specified in development plans, should be replaced by performance criteria, appropriate to location.”

## Urban Development and Building Heights (Draft August 2018)

This policy document takes precedence over any conflicting policies and objectives of development plans, local area plans and strategic development zone planning schemes. Since publication in December 2018 this document has become the only relevant guidance on tall buildings in all planning applications. As such, this application adheres to the guidance within this document, to ensure long term planning compliance and future proofing. This helps the proposals for Clongriffin to align with long term aspirations for Dublin as a sustainable city.



Clongriffin Building Heights



## 2.6 Town Centre Designation

Clongriffin is an emerging town and this application treats it as such. This designation allows opportunities for increased density, reduced parking provision and the provision of Build to Rent (BTR) on a large scale. Being designated a town centre also creates a responsibility to provide a sufficient quantum and quality of commercial development, social and community services, high quality public realm, active frontages and a legible street network. In the preparation of this planning application the design team have been cognisant of this necessity and have designed accordingly.

### Clongriffin-Belmayne LAP

This LAP sets out clearly its aspiration for the creation of a new town at Clongriffin, including the creation of a “strong legible urban structure”. Clongriffin is identified as a location for commercial provision and taller buildings in proximity to the station.

Adherence to specific policy requirements of the Clongriffin-Belmayne LAP are set out in the Planners Report included with this application.

### Dublin City Development Plan 2016-2022 – KDC 1

Clongriffin is identified as KDC1 in the Dublin City Development Plan 2016-2022. This application takes account of this classification and has been designed to incorporate the key principles of District Centres which are;

- An increased density of development
- A viable retail and commercial core
- A comprehensive range of high-quality community and social services
- A distinctive spatial identity with a high quality physical environment

**Population;** A diverse and significant population already exists in Clongriffin and this application will support this further. Earlier phases have focused on individual family homes, which has helped to create a strong base of residents, many of whom are committed to the location through purchasing homes and enrolling children in local schools. This application will bring more apartments, including Build to Sell (BTS) and Build to Rent (BTR) units, which will be attractive to a broad range of family types from those starting out, through families renting in the long term and seeking security of tenure to pensioners who wish to live close to family in well serviced and maintained communities.

**Density;** The proposed development subject to this application SHD 2 provides for a high-density development of 500 no. units which provides for a density of 200 units per hectare. The overall density of development subject to the concurrent applications (i.e. 1,950 units on

the masterplan site area of 11.4 ha) provides for a density of 171 units per hectare. The overall density across the entire Clongriffin Area including the constructed, permitted and proposed units provides for 84 units per hectare (4,518 units on an overall site area of 53.56 hectares). The proposed density is in excess of the recommended minimum of 50 units per hectare as advised under Section 28 Ministerial Guidelines.

The subject site is located on a high-quality transport route and is located in an existing centre within an urban context and therefore has the capacity to achieve higher densities. The proposed density is therefore considered appropriate due to the site’s location adjoining high-quality transport corridors

The density of the proposals is above average for Dublin and takes account of the proximity to public transport as well as the city itself. This density has been achieved through clever design and phasing of development. This will allow a critical mass of footfall to commercial and retail units in the Town Centre. This footfall creates a virtuous circle of customers and demand for businesses opening in the town, and should allow the emergence of a sustainable and vibrant local centre for residents to enjoy.

**Transport;** Clongriffin Station and multiple Dublin Bus routes serve Clongriffin. A commuter car park has been provided at the train station. In addition, provision has also been made for cycling with segregated cycle paths on main roads.

**Commercial/ Retail;** Active frontages are incorporated along all main roads to animate the streetscape and provide passive surveillance. Significant commercial floor area is planned with proposals including multiple retail units, restaurants and a cinema. The diversity of uses available will add healthy competition and options for residents. These commercial uses will also draw residents from surrounding areas, further increasing footfall for local businesses.

**Community & Social Services;** Multiple community and social services have been delivered in the town centre in response to demand to date. Further provisions included in this application include a creches, a community centre and a Mens’ Sheds.

**Employment;** Offices and commercial units are proposed in several blocks located close to Clongriffin Station. Smaller retail units, management companies, landscaping and other industries will also create employment directly associated with the proposals. In addition there will be significant numbers of construction related jobs created on both the short and longer term in the area in relation to the construction of the proposals and ongoing maintenance and renovations of units.

**Built Environment;** A network of streets and spaces of different scale and feel is core to the proposals. These include green spaces, hard landscaped squares and tree lined streets. The majority of public open space has already been provided or is currently on site.



**Sustainable Urban Housing:** Design Standards for New Apartments (March 2018) Clongriffin qualifies as a Central/ Accessible Urban location according to these guidelines as it fulfils the criteria of “Sites within easy walking distance (i.e. up to 5 mins or 400-500m) to/ from high frequency (i.e.min 10 minute peak hour frequency) urban bus services”. The number 15 bus currently runs at a frequency of every 8 minutes (i.e. 7.5 buses per hour) during the morning peak. This frequency is likely to increase in line with the increased population.

### Public Transport (proposed)

The Transport for Ireland (TFI) Bus Connects will have a positive impact on Clongriffin if it is successfully implemented as currently planned. The proposals place Clongriffin at the start of Core Bus Corridor 1. This will result in continuous bus and cycle priority from Clongriffin to the city centre. This will only improve connections for residents of Clongriffin over the longer term.

# 15

Buses from/to  
From Clongriffin To Ballycullen Rd.  
Operative Date: 21/10/2018  
Version: TT 8.2

From Clongriffin Towards Ballycullen Rd.

[View on Map](#)



Clusin Ghriffin, Timpeallán Ard Aithin, Fionnradharc, FSC, Sráid Aungier (Sráid an Easpáig), Ráth Maonais, Tír an Iúir, Droichead Theach Meaóg, Bóthar Bhaile Uí Chuilín

Buses leave terminus at:

	Monday-Friday	Saturday	Sunday
	05:00 05:30 06:00 06:10	06:00 06:15 06:30 06:45	08:00 08:30 09:00 09:20
	06:20 06:30 06:40 06:51	then every 15 minutes until 21:30	09:40 10:00 10:20 10:40
	07:02 07:10 07:18 07:26	21:45 22:00 22:15 22:30	11:00 11:20 11:40 12:00
	07:30† 07:34 07:42 07:46†	22:45 23:00c 23:15c 23:30c	then every 15 minutes until 21:45
	then every 8-12 minutes until 19:00		22:00 22:15 22:30 22:45
	19:15 19:30 19:45 20:00		23:00c 23:15c 23:30c
	20:15 20:30 20:45 21:00		
	21:15 21:30 21:45 22:00		
	22:15 22:30 22:45 23:00c		
	23:15c 23:30c		

Clongriffin (Sq.) • 20mins • Artane Roundabout • 15mins • Fairview • 8mins • FSC • 13mins • Aungier St. (Bishop St.) • 10mins • Rathmines • 14mins • Tereure • 5mins • Templeogue Bridge • 15mins • Ballycullen Rd.





An increased density of development

A viable retail and commercial core

A comprehensive range of high-quality community and social services

An increased density of development

A distinctive spatial identity with a high quality physical environment





## 3.0 MASTERPLAN & URBAN DESIGN

### 3.1 Urban Framework Plan & Design Strategy

The Urban Framework Plan in Clongriffin is, and has always been, to create a sustainable, high density, vibrant, mixed-use town centre. The Framework Plan is an overall vision for Clongriffin Town Centre and includes earlier phases, the subject application and concurrent applications. The structure of Clongriffin was set up in the original planning application in 2003 and no deviation from this is being sought. The Framework Plan takes its cues from the Clongriffin-Belmayne LAP Urban Design Section. Compliance with this document is detailed in the Clongriffin Masterplan Document (June 2018, copy included with this application) and will not be repeated here.

The Urban Structure of Clongriffin focusses around a series of public open spaces, both green and hard landscaped, connected together through a highly permeable network of streets. The streets themselves have a hierarchy, with vehicular traffic encouraged to the perimeter roads. Pedestrians and cyclists are offered more options between largely segregated routes, such as through the Mayne River Park and Market Street link, to quieter neighbourhood streets shared with vehicles but designed to be low traffic. On-road cycle tracks are also provided on the strategic perimeter roads for fast moving cyclists who simply want to get from A to B.

The town structure sets up a series of 'zones', ranging from the commercial centre, focussed around Station Square and the DART station, through high density 'urban living' in perimeter blocks with strong streets edges adjacent to the commercial centre, to medium density single family homes closer to Fr. Collins Park and Beau Park. These zones are not hard and commercial and community uses are dotted throughout the town for convenience and practicality. Commercial uses are also consolidated along Main Street and Station Street, where heavier footfall will help support small business and local services.

Vistas and views have been considered with several key views fundamental to the structuring of the town. These include the view down Main Street, towards Clongriffin Station. As identified in the Clongriffin-Belmayne LAP this vista will be closed by Block 17, a 17 storey tower. This building is an important marker, signifying not only the station itself,



Station Square Sunday market

but the presence of the emerging commercial centre to Clongriffin. This will be important in the longer term for the viability of major commercial uses in the town centre and will serve as a landmark for the town. Block 17 is subject to a concurrent application.

Legibility and wayfinding have also been carefully considered and each block in the subject application addresses these issues locally, whilst fitting into a wider network of legible streets. Broadly, strong street edges have been encouraged, with active uses and frontages as much as possible. Where commercial frontage is not practical, ground floor residential is typically proposed. These vary from own-door and internally accessed units. Each block in Clongriffin SDH 2 will be built from a complimentary palette of hard-wearing, durable materials, such as brick, aluminium and stone cladding. Whilst the palette is similar, each block will differ somewhat in style, with these variations adding to legibility as roof planes, corners, open spaces, commercial units etc all work together to create local distinctions.

Overall the Urban Framework Plan aims to set up a family-oriented, sustainable and viable town centre in Clongriffin. Earlier phases have included lower density homes and significant public open space provision. Clongriffin Planning Application, and the two concurrent applications working alongside it, will provide the density and structure that is still missing, to complete the vision for Clongriffin and finally deliver a self-sustaining and vibrant town centre.

Extract from  
Clongriffin-Belmayne  
LAP

It is the objective of the Council:

- UDO1** To achieve high quality and sustainable densities to consolidate the area, maximise access for residents and employees to public transport and successfully define important locations and routes including the Main Street access and town centres (KDC designated zones).
- UDO2** To promote the development of family orientated, high quality, adaptable, life long homes within the LAP area through creative design that still delivers a sustainable density to support the provision of services.
- UDO3** To promote public spaces that provide both passive and active recreation and a connection to those forms of activities in the surrounding area.
- UDO4** To ensure high quality streetscapes are achieved through distinctive high quality street furniture, lighting, paving and public artwork that creates a distinctive character associated with the North Fringe.
- UDO5** To design park spaces and all open spaces linkages to be part of the larger green network in particular from Father Collins Park to the surrounding area and along the River Mayne incorporating a dedicated network of cycle paths and pedestrian routes.
- UDO6** To achieve creative and unique character through public realm enhancements in all key development sites through development contribution levies and partnership with the divisions of Dublin City Council.
- UDO7** The height strategy for the LAP will seek positive integration of new building height with established character. Locations identified for special height character are the designated Key District Centres (in general 5 storeys minimum) and the Main Street Boulevard axis (in general four to five storeys). Heights of 2-6 storeys (including a set back at the top floor of a 5/6 storey building) may be facilitated subject to quality design criteria and set back requirements along the river corridor to complete the urban form of pavilion buildings to complete

Marrsfield, One location for a landmark profiled building (10-14 storey office height equivalent) is designated adjacent to Clongriffin Rail Station. In other locations, where 4 storeys residential height is proposed, some flexibility will be allowed on the height equivalent (13m) to achieve design improvements to the façade.

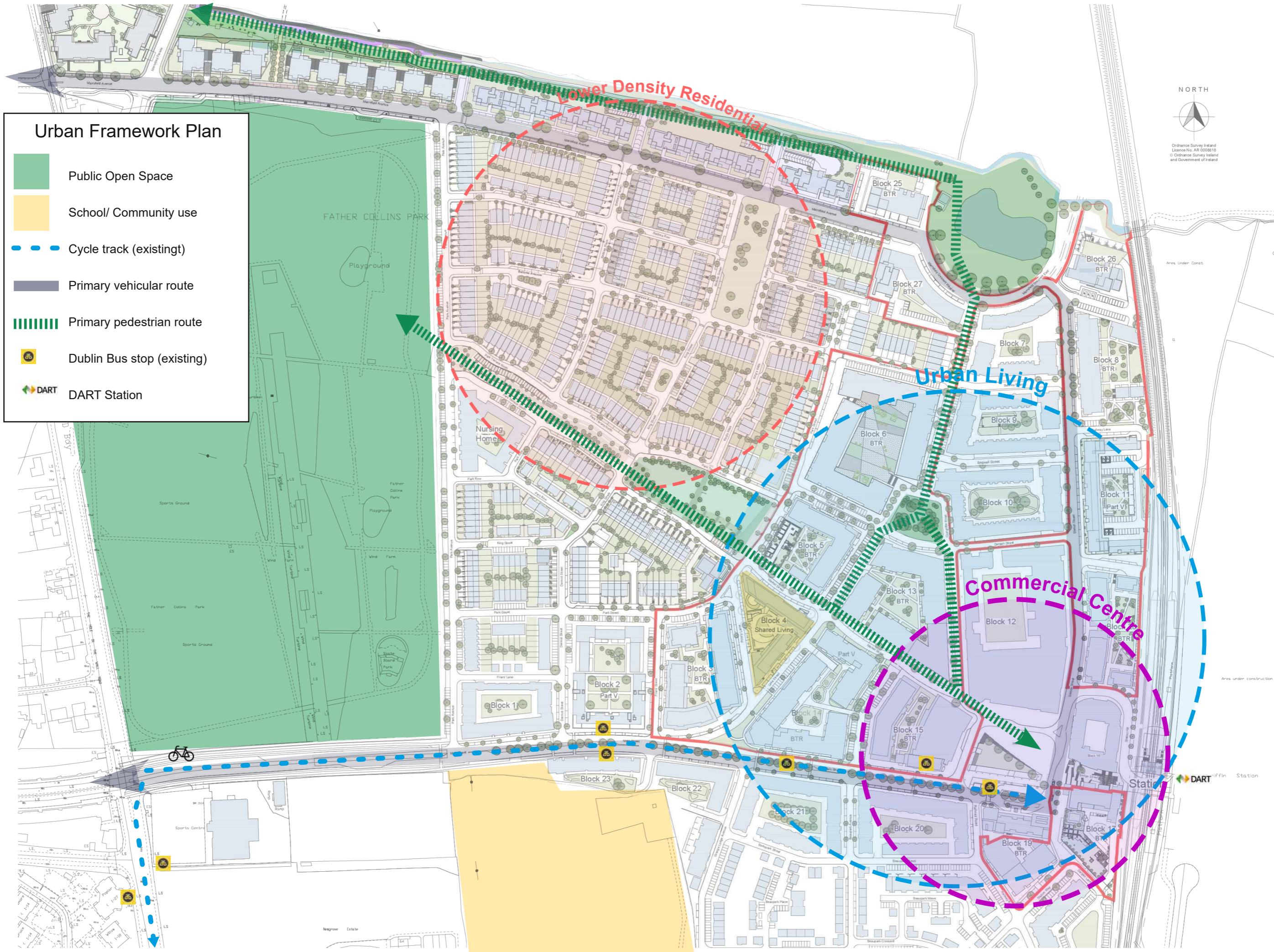
- UDO8** To seek full compliance within all permitted developments on planning conditions, completion of infrastructure, adherence to phasing policy, taking in charge standards building control and fire regulations and seek more effective mechanisms for Dublin City Council to seek confirmation of full compliance on all planning, design and build stages for future developments in the LAP area.

- UDO9** To seek and assess the satisfactory arrangements for the future management of multiple unit developments as an integral part of a scheme planning proposal to ensure that residential amenities are protected in the management of completed developments in accordance with city policy of encouraging lifetime homes.

- UDO10** To minimise the adverse impacts of noise and promote good health and a good quality of life through the effective management of noise within the Clongriffin-Belmayne Local Plan Area.

- UDO11** To preserve and maintain good air quality in the plan area in accordance with national and EU policy directives on air quality and where appropriate promote compliance with established targets





### Urban Framework Plan

- Public Open Space
- School/ Community use
- Cycle track (existing)
- Primary vehicular route
- Primary pedestrian route
- Dublin Bus stop (existing)
- DART Station



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 and Government of Ireland

Area Under Const.

Area under construction

DART Station



## 3.2 Movement & DMURS

The Clongriffin Masterplan was laid out in accordance with the principles of DMURS and previous best practice and guidance in this area. The four Key Characteristics of Place Based Design are core to the proposals and Clongriffin SHD 2 fully supports these aspirations.

Achieving better street design in urban areas will encourage more people to choose to walk, cycle or use public transport by making the experience safer and more pleasant. It will lower traffic speeds, reduce unnecessary car use and create a built environment that promotes healthy lifestyles and responds more sympathetically to the distinctive nature of individual communities and places. Clongriffin is intended to deliver a high quality development which complies with the recommendations of DMURS and Clongriffin SHD 2 helps support this aim.

The proposed development has been designed to reduce traffic speeds with long straight sections of road, where possible, being avoided. Road junctions incorporate raised tables which improve pedestrian crossing facilities, particularly for disabled users and people pushing prams/ buggies, and serve as an additional traffic calming measure.

DMURS sets out four core design principles which designers must consider in the design of roads and streets.

### Connectivity

Permeability and movement are key to the layout and usability of the town centre. A hierarchy of routes has been created, which will encourage traffic of each mode onto the appropriate route, whilst outright barriers to movement have generally been avoided. The provision of high quality, direct links, will encourage active modes of transport, with cycling and walking taking precedence for 'last mile' and local journeys.

The proposed development has been carefully designed so that the private car does not enjoy the level of connectivity afforded to pedestrians and cyclists. Journey times and routes for car-based transport are considerably longer and more cumbersome in order to make it more attractive for walking and cycling to the local shops and schools. The development is, however, well connected to the surrounding road network.

The masterplan is designed as a permeable neighbourhood with a hierarchy of streets from neighbourhood to regional. This allows pedestrian and cyclists options, depending on their journey purpose and confidence. Completed main roads include segregated cycles facilities and generous footpaths. Most of the development's internal road network has been constructed under the parent planning application, Reg. Ref. 0132/02, PL29N.131058. The internal road network connects with Marrisfield Avenue to the north and Main Street to the south, both of which connect to Hole in the Wall Road to the west of the development.

Though construction of the main road network is substantially completed, additional road treatments are proposed. The proposed development is to be compliant with the recommendations set out in the Design Manual for Urban Roads and Streets (DMURS), the stated objective of which is to achieve better street design in urban areas.

A new link has been provided under the railway in the NE corner of the lands. This will connect the River Mayne Linear Park through to Baldoyle Nature Reserve and provide a new option for movement within and through the lands.

### Enclosure

The proposed blocks all propose strong street edges, with building lines coming forward to the back of pavement where commercial uses are proposed at ground floor. Where residential units are proposed at ground floor, a minimum of 2m 'defensible' zone is proposed, with these spaces carefully designed to facilitate residential privacy whilst maintaining 'eyes on the street'.

The use of street trees can also enhance the feeling of enclosure. The proposed development has been designed so residential units are overlooking streets and public open spaces, which provides passive surveillance. Landscaping and tree planting are provided along the roads/streets which assists in providing a sense of enclosure and enhanced street environment.

### Active Edge

Each block fronts directly onto the surrounding roads and streets. Entrances to blocks are provided directly from the street in addition to proposed commercial and retail units. Additionally, own-door residential units will increase footfall and activity.

Glimpsed views into internal spaces are also provided in certain blocks, including Block 15. This adds dynamic and interest to the streetscape.

### Pedestrian Facilities/ Activity

The sense of intimacy, interest and overlooking that is created by a street that is enclosed and lined with active frontages enhances a pedestrian's feeling of security and well-being. Good pedestrian facilities (such as wide footpaths and well designed crossings) also makes walking a more convenient and pleasurable experience that will further encourage pedestrian activity. All footpaths proposed are a minimum of 2m wide, typically wider, with street planting, car parking, bicycle stands and other uses added to create diversity.

Two pocket parks are included in a concurrent application, adjacent to Block 6 and Block 8. These spaces improve the pedestrian realm and add resting opportunities and shading. The pedestrian priority street along Market Street is included within this application, as it runs alongside all three Blocks 4, 5 and 14.

Both perpendicular and parallel on-street parking spaces are incorporated at various locations throughout the development. On-street parking separates pedestrians from the vehicle carriageway and, as per DMURS Section 4.4.9, can calm traffic by increasing driver caution, contribute to pedestrian comfort by providing a buffer between the vehicular carriageway and foot/cycle path and provide good levels of passive security.

Roads running north-south through the development have been designed with a gentle horizontal curvature, which helps to calm traffic without impeding on safe sight lines or unduly increasing walking distances for pedestrians, and is in accordance with alignment and curvature recommendations set out in DMURS Section 4.4.6. The public areas fronting and within the proposed development have been designed by the multidisciplinary design team to accommodate pedestrians and cyclists in accordance with the appropriate principles and guidelines set out the Design Manual for Urban Roads and Streets.

It is considered that the proposed development is fully compliant with the connectivity objectives of DMURS.

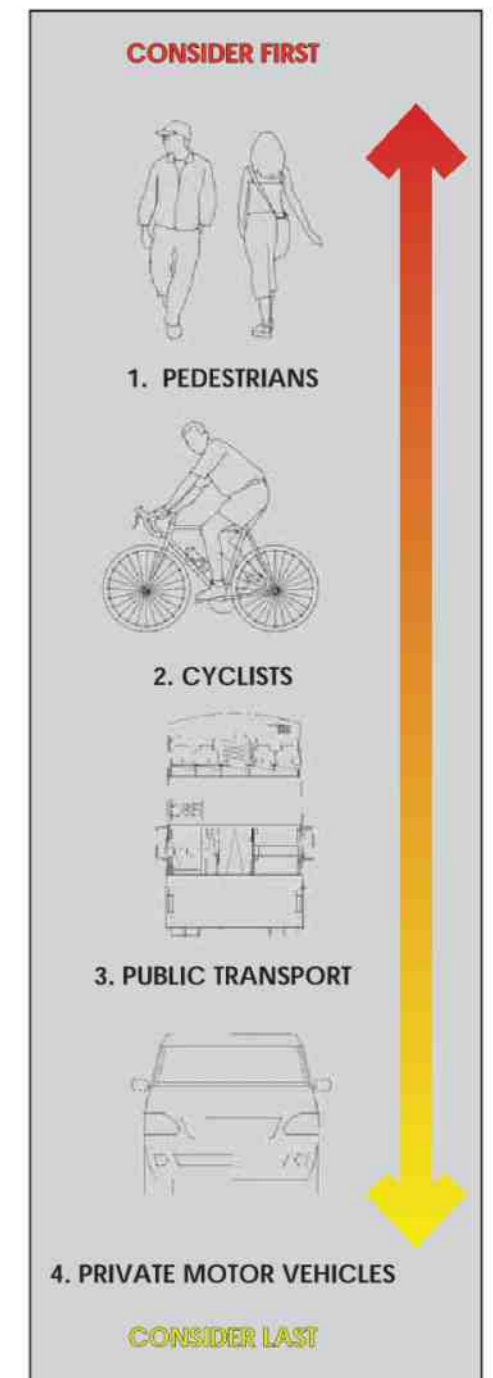
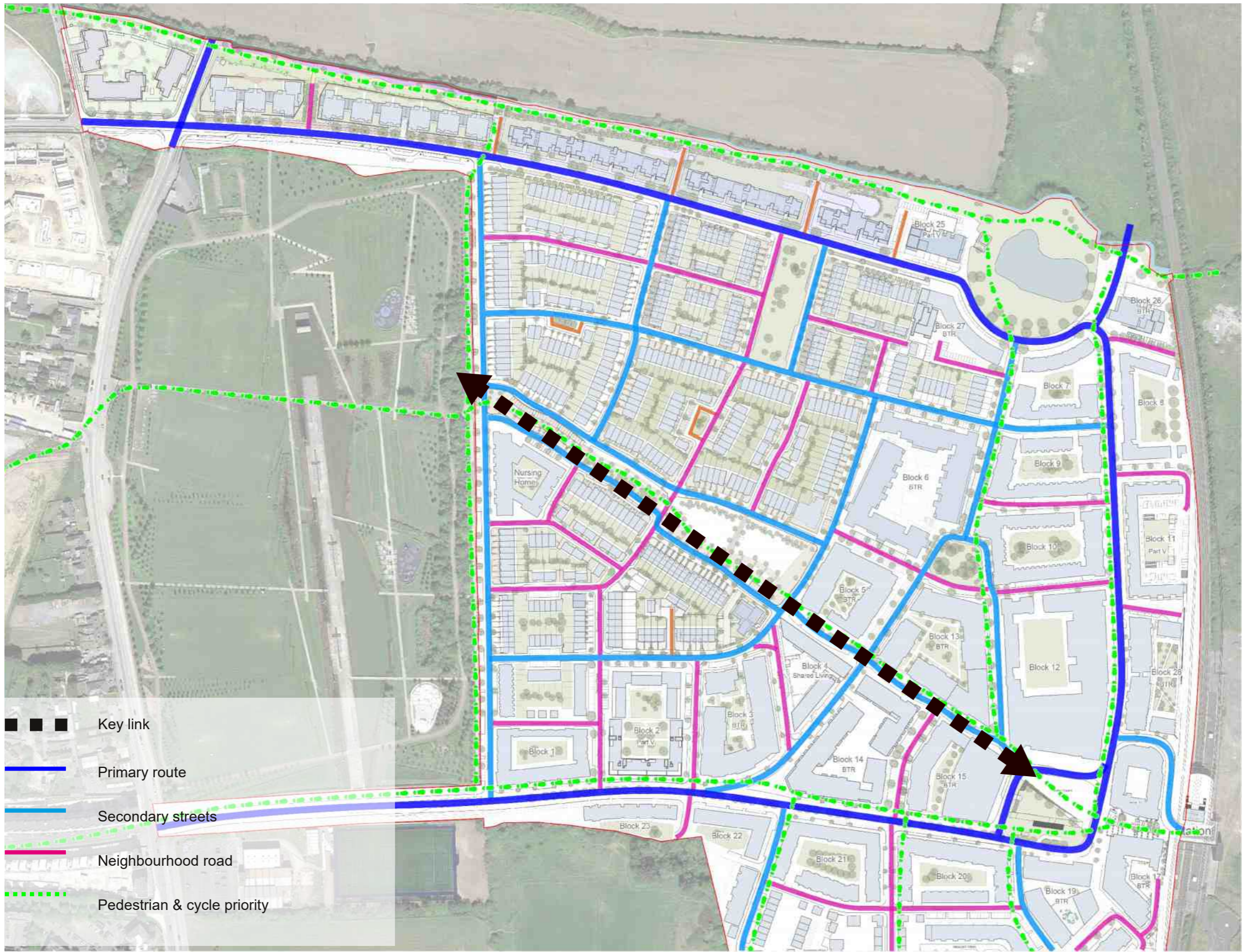


Figure 2.21: User hierarchy that promotes and prioritises sustainable forms of transportation





- ■ ■ Key link
- Primary route
- Secondary streets
- Neighbourhood road
- - - Pedestrian & cycle priority

Hierarchy of streets within the masterplan



# 3.3 Connections & Green Links

Central to the Clongriffin Masterplan are several green links and connections that link together a series of small and medium sized open spaces and create an environment where walking and cycling are the most attractive mode choices.

The primary connection is the 'String of pearls' route from Father Collins Park to Station Square. This link has been carefully designed to encourage pedestrian and cycle priority and craft a journey that moves through spaces and streets of different character. Three public spaces – Fr. Collins Park, Belltree Green and Station Square - are connected by high quality public realm.

The journey leads from the large scale Fr. Collins Park, through a quiet and tree-lined street, to the intimate but more urban Belltree Park, with it's smaller scale and plentiful benches. Then it continues along a hard landscaped, shared surface, pedestrian and cycle priority street, to the town centre and urban environment of Station Square.

Views between each space can be glimpsed along the way and a similar language of street furniture ties each section of the route together through lighting, benches, tree species and hard landscaping.

The subject application is central to the success of this route as all three blocks have active frontage onto Market Street. The success of these blocks will be crucial to this important link, as activity along this street will improve the pedestrian realm, create a feeling of security and lead to an improved sense of community, urban context and amenity. The three blocks are set up to provide passive surveillance and activity all through the day and evening, with active uses at ground floor, primary pedestrian entrances to apartments and 'eyes on the street' from apartments above.

This street is designed not only for consuming; tree planting, public benches, street lighting and community uses are also located here to encourage informal usage, lingering and chance encounters. Bicycle parking is also provided for convenience.

Two pocket parks are proposed in a concurrent application; Grant Park on Lake Street and Railway Park adjacent to Block 8. Both of these pocket parks will be important in their own right for pedestrian connections and green links within Clongriffin. Grant Park, in particular, will be an important node on the link from the River Mayne Linear Park to Station Square.

Further detail on active uses be found in the individual block reports and full scale drawings. Further detail on the design of Market Street can be found in the Landscape Architects report and full scale drawings.

Neighbourhood Street with greenway



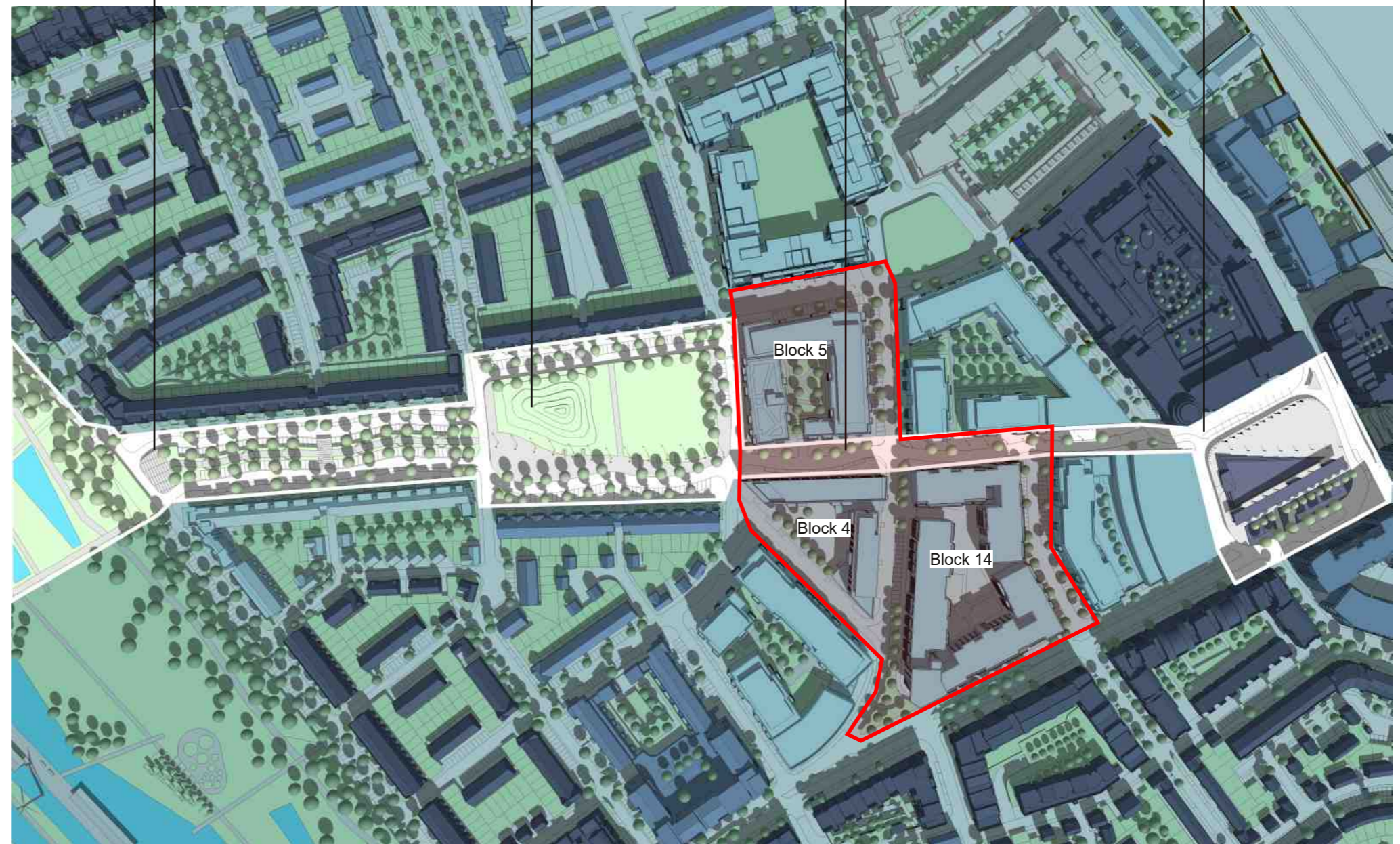
Bellpark Square



Pedestrian & bicycle priority street



Station Square







Recreation space at Fr. Collins Park



The Attenuation Lake and River Mayne Linear Park, July 2018

A network of pedestrian and cycle friendly routes permeate the Town, linking up small pocket parks, the large regional park and the urban Station Square



# River Mayne Corridor

Another green link that is being provided and which is a feature of the Clongriffin-Belmayne LAP is the River Mayne Corridor.

The River Mayne corridor has been partially completed with developments infilled along Marrsfield Avenue and construction underway at Riverside and down as far as the attenuation lake and Wetland Park at the junction with Dargan Street.

Landscaping along the river is high quality and a network of pedestrian footpaths has been linked along the full length. Extensive tree planting, of native species, in addition to smaller shrubs and plants have also been provided.

A new pedestrian link has been provided in the northeast of the site, under the railway, to connect the River Mayne Park into the Baldoyle Nature Park.

Blocks within concurrent applications will provide important context for this park as they create enclosure and passive surveillance, overlooking the linear park and attenuation area. These blocks have been laid out to encourage active use of the open space, provide passive surveillance and give a structured edge to the open space.

Darndale Park



River Mayne Corridor



Father Collins Park



Attenuation pond and Wetland Park under construction



New link provided under railway to Baldoyle Nature Park



Apartments approved at Marrsfield Avenue beside Fr. Collins Park

Apartments completed at Marrsfield Avenue

Pedestrian path and new planting along River Mayne

River Mayne Corridor



The River Mayne Corridor links several green spaces and parks on its journey to the coast



## 3.4 Urban Form

A hierarchy of streets has been provided for with one primary vehicle route around the outer edge of the masterplan. This road, Station Street and Marrsfield Crescents East and West, are included in a concurrent application, effectively completing frontage onto this primary road network, for all sites within the applicants ownership.

An internal network of smaller scale, neighbourhood roads provides alternative, slower, options for cyclists and local traffic. A central spine links Fr. Collins Park with Station Square along a pedestrian and cycle priority road. Market Street, within the subject application, makes up a significant portion of this link, improving connectivity for pedestrians.

Different road widths, on-street parking, tree planting and landmark buildings have been employed to aid legibility and wayfinding, whilst slowing traffic. Emphasis has been made to create pedestrian and cycle friendly neighbourhoods, utilising principles from DMURS.

Main streets have residential units above commercial and community facilities at street level. This allows 'eyes on the street' and passive surveillance. Within this application all three blocks have some commercial uses at ground floor. They are all located close to the commercial centre and primary routes.

A network of social spaces is also provided, from the large expanse of Fr. Collins Park, through formal play areas for children, local green kickabout areas, informal green spaces to steps and benches that encourage lingering and informal socialising. These include the two pocket parks within concurrent applications and the open space along Market Street, within this application.

A variety of building types have been provided and allowed for in the masterplan. Typologies vary from two and three storey terraced homes on neighbourhood streets, to higher density apartments and duplexes in shared and mixed-use blocks. As earlier phases have focussed on building medium density, single family homes, the subject application proposes a higher density of perimeter blocks of apartments. This allows an average density sufficiently high to sustain a mixed-use town centre.

The form of each block responds to it's location and function within the masterplan, with density and height used to structure civic spaces and aid wayfinding and legibility of streets. Taller corners and active frontages are utilised to structure the masterplan, provide edges, nodes and structure. Block 14, in particular, makes use of increased height as a space structuring device. Its massing helps to define the important junction of Market Street and Lake Streetm where it is located. This aids wayfinding, legibility and provides a landmark.



Figure Ground Now



Figure Ground with Clongriffin SHD 2



Figure Ground with Clongriffin SHD 1 and concurrent applications



## 3.5 Public Realm

The public realm within the masterplan is varied and adapts to the context. In residential areas principles from DMURS have been employed to slow traffic and accommodate private parking in a way that improves the street environment. Within the subject application roads are a mix of the strategic vehicular route along Lake Street, smaller access roads including Park Lane and the primary pedestrian route of Market Street.

On-street parking, incidental open spaces, tree planting and lighting have all been employed to enliven the street environment and subtly demarcate between private and public areas. On-street parking spaces will incorporate SUDS measures and street trees will help to soften the feel of the streets.

On primary routes, and in Station Square formal lighting standards are used, in conjunction with high quality finishes, to give a civic feel. Level differences are exploited to allow steps, ramps and planters to provide informal resting and lingering spaces. Steps along the boundary of Block 17 (in concurrent application), approaching the station, add dynamic and unexpected urban streetscape. High quality finishes and lighting along Market Street will feel urban in nature.

Taller buildings, such as Block 14, with flush frontages to the street, provide civic structure whilst smaller elements are incorporated to introduce a human scale and help people navigate. The west side of Block 4 is an example of a smaller scale, with Mens Shed and creche located at ground level with only one floor above.

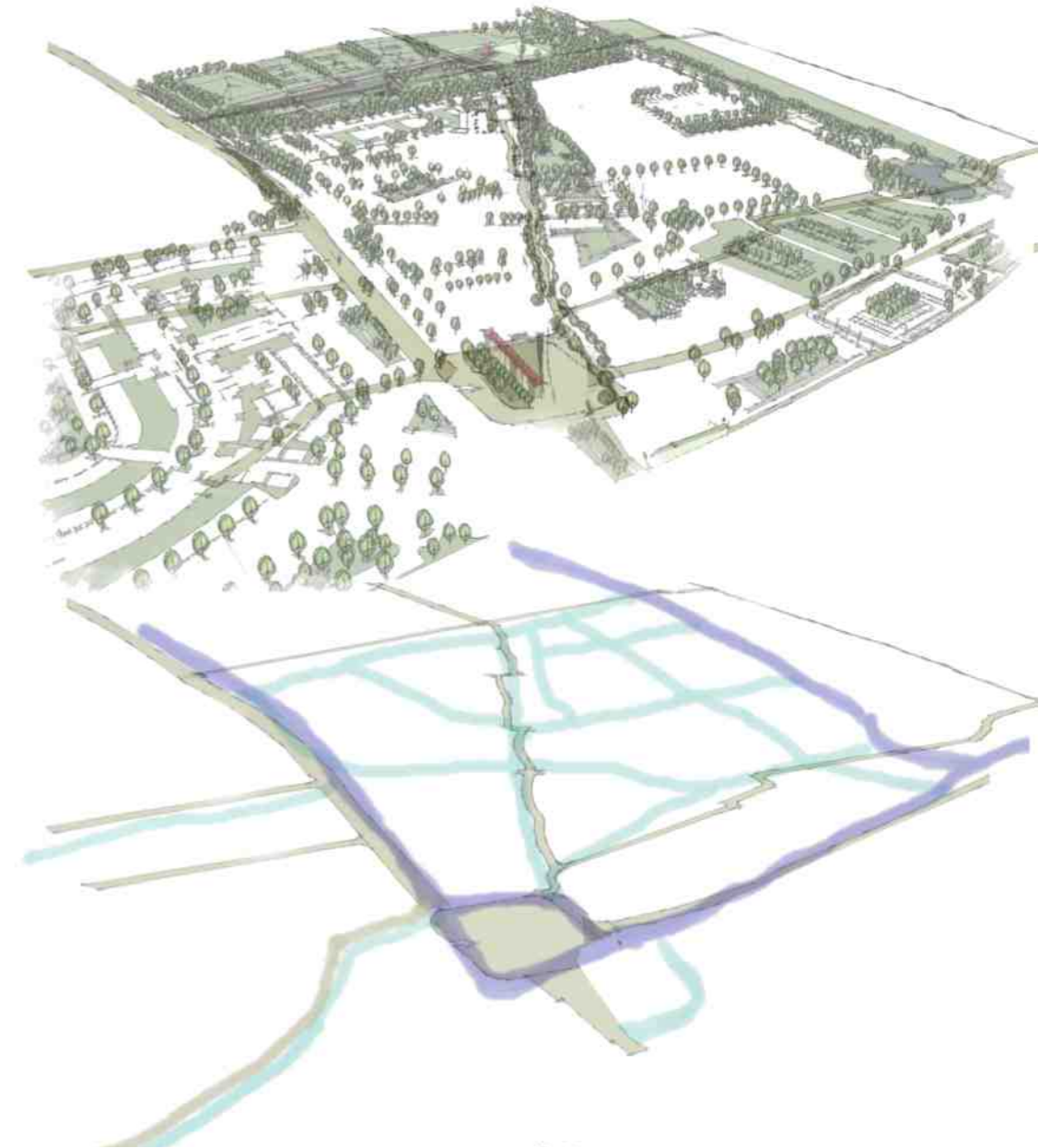
Glimpses views into semi-private spaces will also add diversity to the street experience, for example Block 14 opens up on each side to allow views to the internal space from surrounding streets.



View along Main Street looking east

### Landscaping;

A lush landscape of street trees, pocket parks and high quality plazas layers on top of the public realm to create a green and sustainable environment. Internal landscaped podiums contrast with public parks and pockets parks, to allow glimpses of greenery from almost all vantage points. The new town also sits into a wider landscape of parks and open spaces, all linked and easily accessible from Clongriffin.



### Public Realm;

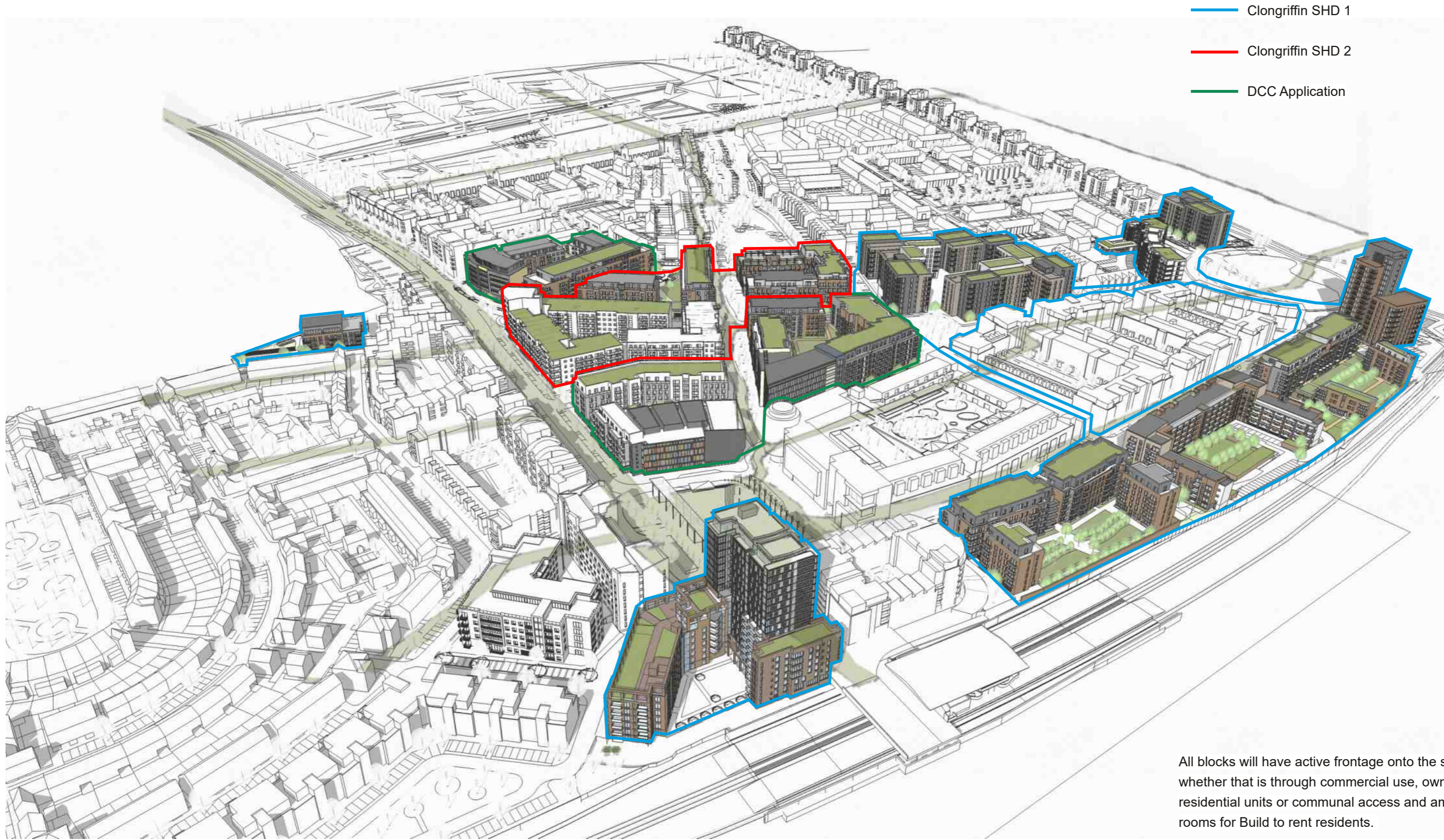
The public realm is structured to create a hierarchy of routes; cars and buses will naturally stay on the main routes of Main Street, Station Street and Marrisfield Avenue whilst pedestrians and cyclists can navigate a permeable network of narrower streets, linked together with pocket parks and plazas. The permeability allows options and the network of open spaces aids wayfinding.

### Buildings;

The buildings set up a strong streetscape through the intentional use of active frontages, strong street edges, use of height and features to aid wayfinding and legibility. A combination of own-door units and units accessed from internally with open spaces onto the street creates a diversity of street environment that allows local identity within a legible whole.







- Clongriffin SHD 1
- Clongriffin SHD 2
- DCC Application

All blocks will have active frontage onto the street whether that is through commercial use, own-door residential units or communal access and amenity rooms for Build to rent residents.



# Public Realm (cont.); Market Street

Market Street runs through the centre of this application, providing civic and commercial functions. Market Street will connect Fr. Collins Park to Station Square along a pedestrian and cycle focussed street, with active frontage most of its length.

This pedestrian and cyclist priority street has been carefully laid out to encourage a safe and inviting environment for those on foot or two wheels, with car park entrances located to the sides of adjacent blocks and traffic encouraged onto other roads. Small amounts of traffic will be allowed onto the street as necessary, for example for drop off. Raised tables will be employed at the minor junctions crossing the street to make the hierarchy clear.

High quality landscaping, bicycle stands, tree planting and active frontages from both civic and commercial uses will all combine to create a lively and safe street. Public benches will also be provided to encourage lingering and informal encounters.

Active frontages onto the street, minimal car traffic, limited on-street parking and occasional loading access and passive surveillance from residential units above, all combine to create a balanced, human scale street.

Further detail on active uses be found in the individual block reports and full scale drawings. Further detail on the design of Market Street can be found in the Landscape Architects report and full scale drawings.

On-street parking adds activity



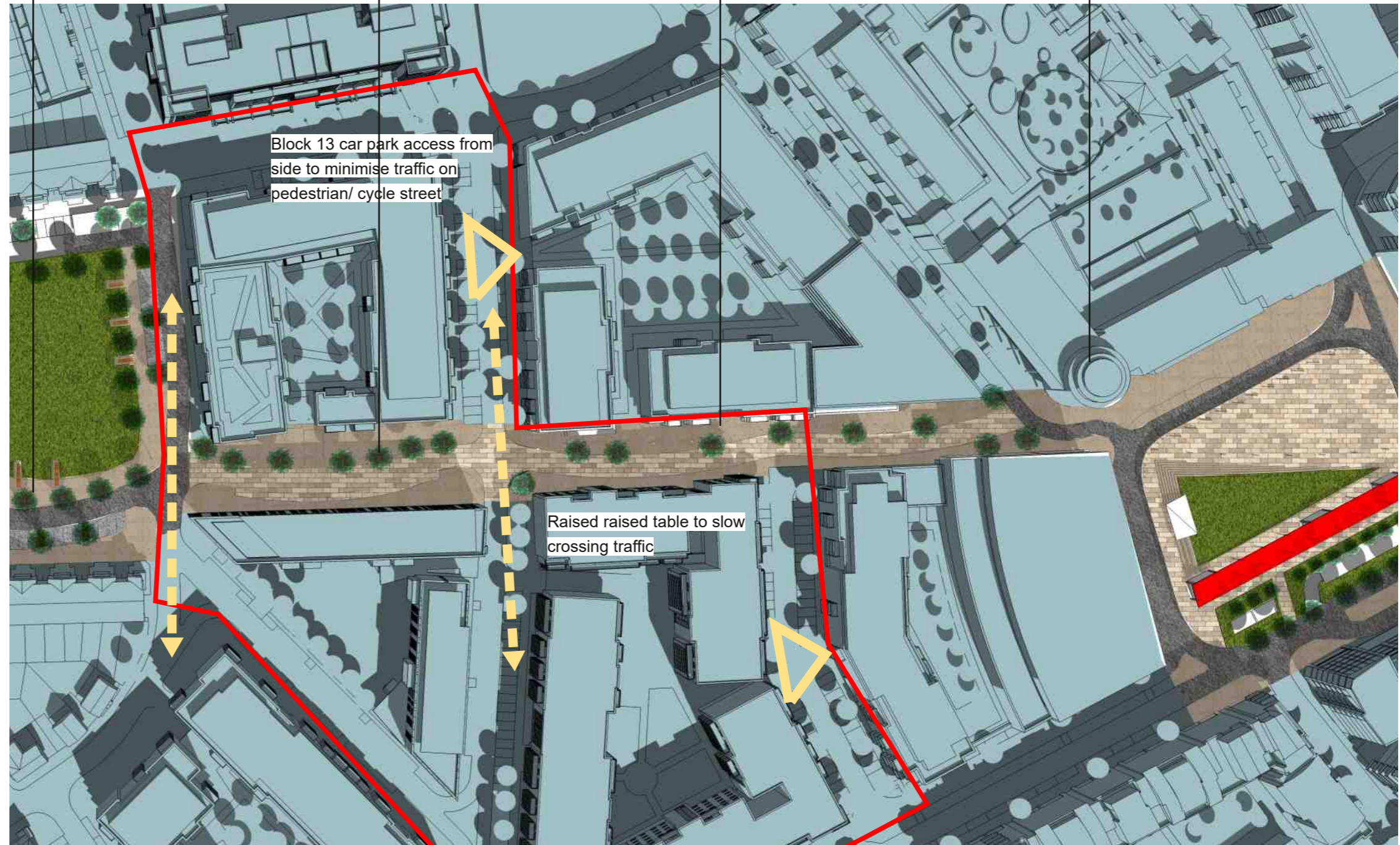
Flush surfaces encourage driver caution



Material finishes indicate priority and segregation



Street lighting choice implies pedestrian scale



Block 13 car park access from side to minimise traffic on pedestrian/ cycle street

Raised raised table to slow crossing traffic

Extent of shared surface/ cycle & pedestrian priority





	<b>PROPOSED TREE PLANTING</b> See list
	<b>GRASS PLANTING</b>
	<b>PROPOSED SHRUB PLANTING</b> See list
	<b>PROPOSED HEDGE PLANTING</b>
	<b>URBAN SOIL TREE PIT</b> 25m <sup>2</sup>
	<b>BOUNDARY TREATMENT</b> Garden kerb 50x150x900mm
	<b>BOUNDARY TREATMENT</b> Rendered Wall 450mm high with Capping 450x600x80mm
	<b>BOUNDARY TREATMENT</b> Charcoal 100x100 double row border 200mm wide
	<b>BOUNDARY TREATMENT</b> 1200mm high Perforated railing/Glass balustrade
	<b>SURFACE TREATMENT</b> Paving flag 600x400 grey (to match existing on Main Street)
	<b>SURFACE TREATMENT</b> Tegula charcoal 175x140x50mm & 140x140x50mm
	<b>SURFACE TREATMENT</b> Sienna silver 208x173x50mm & 173x173x50mm
	<b>SURFACE TREATMENT</b> Private terrace Retro heather 190x50x60mm
	<b>SURFACE TREATMENT</b> Beige tarmac/Dust path
	<b>SURFACE TREATMENT</b> Concrete path, brush finish with trowel edge
	<b>SURFACE TREATMENT</b> Hydropave Pedesta for Car Parking 200x100x60 Brindle with charcoal border (to match existing)
	<b>SURFACE TREATMENT</b> Market St. Guangze granite 200x100

**Proposed Small Tree Planting**

No.	Name	Size
T15	Acer palmatum	150cm
T16	Acer shirasawanum	200cm
T17	Acer griseum	14-16cm
T18	Amelanchier lamarckii	200cm
T19	Arbutus unedo	200cm
T20	Betula pendula	14-18cm
T21	Betula pendula(3 stem min.)	350cm
T22	Betula papyrifera	14-18cm
T23	Ornithodendron hookerianum	50L
T24	Laurus nobilis 'Cone Shaped'	200cm
T25	Laurus nobilis(3 stem min.)	200cm
T26	Rhus typhina	120cm
T27	Salix tortuosa	300cm
T28	Malus 'John Downie'	14-16cm
T29	Viburnum opulus	14-18cm

**Proposed Street Tree Planting**

No.	Name	Size
T1	Platanus acerifolia	18-20cm
T2	Tilia cordata 'Greenspire'	16-18cm
T3	Tilia cordata 'Green column'	16-18cm
T4	Acer campestre 'Elsrijk'	16-18cm
T5	Pyrus calleryana 'Chanticleer'	16-18cm
T6	Quercus palustris	16-18cm
T7	Prunus avium 'Plena'	16-18cm
T8	Sorbus aria	16-18cm

**Proposed Planting A**

No.	Name	Size
H1	Elaeagnus x ebingel	3L
S1	Lavandula angustifolia	3L
S6	Cornus alba	2L
G1	Bergenia cordifolia	1L
G4	Hedera helix Hibernica	1L
P1	Erysimum 'Bowle's mauve'	3L
P7	Verbascum spp.	1L

**Proposed Planting B**

No.	Name	Size
H4	Laurus nobilis	3L
S3	Cistus corbariensis	3L
S8	Hypericum hidcote	2L
G3	Geranium 'Rozanne' (Jolly bee)	1L
G9	Rubus tricolor	1L
P2	Verbena grandiflora	2L

**PROPOSED FLUSH KERB**  
Silver granite surface drain

**SEATING**  
Omos or similar approved

**REVISIONS:**

Rev	By	Date	Description
Rev A	MR	17.08.18	Revised Layout
Rev B	MR	05.09.18	Revised Layout
Rev C	MR	19.09.18	Revised Layout
Rev D	MR	31.10.18	Revised Layout
Rev E	MR	15.11.18	Revised Layout
Rev F	MR	26.11.18	Revised Layout
Rev G	MR	03.04.19	Revised Layout
Rev H	MR	12.07.19	Revised Layout

Project Title: Proposed Market Street, Clongriffin, Dublin 13  
 Client: Gerard Gannon Properties  
 Drawing Title: Market Street  
 Job No.: 1147 D

Dwg. No.: 01  
 Scale: @: A1  
 Date: 07.08.18  
 Drawing By: MR  
 Checked By: RMD

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## 3.6 Neighbourhoods & Street Naming

Appendix 3 of the LAP sets out the following aspirations for larger schemes;

*New developments should promote or reinforce local distinctiveness without stifling innovation. The best schemes are usually those that recognise the individuality of a place, and either tailor standard solutions or create particular and original architecture for that site.*

New typologies have been introduced across Clongriffin to create a distinctive neighbourhood that has its own character, whilst sitting comfortably in the context.

Earlier phases of development in Clongriffin have introduced medium density housing, in terraces of two and three storey single family homes. These have been laid out with shared on-street parking, plentiful public open space and generous footpaths and public realm. These typologies have set the scene for a higher density style of living, with all the benefits of increased amenity and safer environment that density brings.

Along Main Street, the precedent has been set for perimeter blocks with commercial uses at ground floor. The subject application will continue this theme, with the benefit of having learned from earlier buildings so lessons have been incorporated into this and concurrent phases. This includes some changes to the material palette, with preference for highly durable materials such as brick and metal composites.

Approximately half of units proposed within this application will be Build to Rent, which is a typology not prevalent in Dublin, particularly this part of Dublin, at present. This typology will distinguish homes in Clongriffin, with high quality, highly serviced homes available for rent, with resident amenity integrated to the design from the outset. Resident support facilities, such as concierges, will make living in Clongriffin highly convenient.

Several distinct neighbourhoods exist within the Clongriffin lands, providing options to home buyers and renters and adding interest to the area. These vary from terraced home, through smaller apartment blocks to higher density, perimeter blocks such as Block 14. Tenure options will also vary across the town, from home-owners to short and longer term renters. A large proportion of social housing homes are also provided within the town, integrated into the proposals. These include the northern portion of Block 14, within this application.

The masterplan sets up a framework of neighbourhoods within which communities can grow. Appropriate community facilities and services have been provided for and are covered in other sections of this report.

All public spaces have been designed in accordance with principles from the Urban Design Manual: A best practice guide published by the Departments of Environment, Heritage and Local Government. These spaces are spread throughout the town and two fall within the boundary of the subject application.

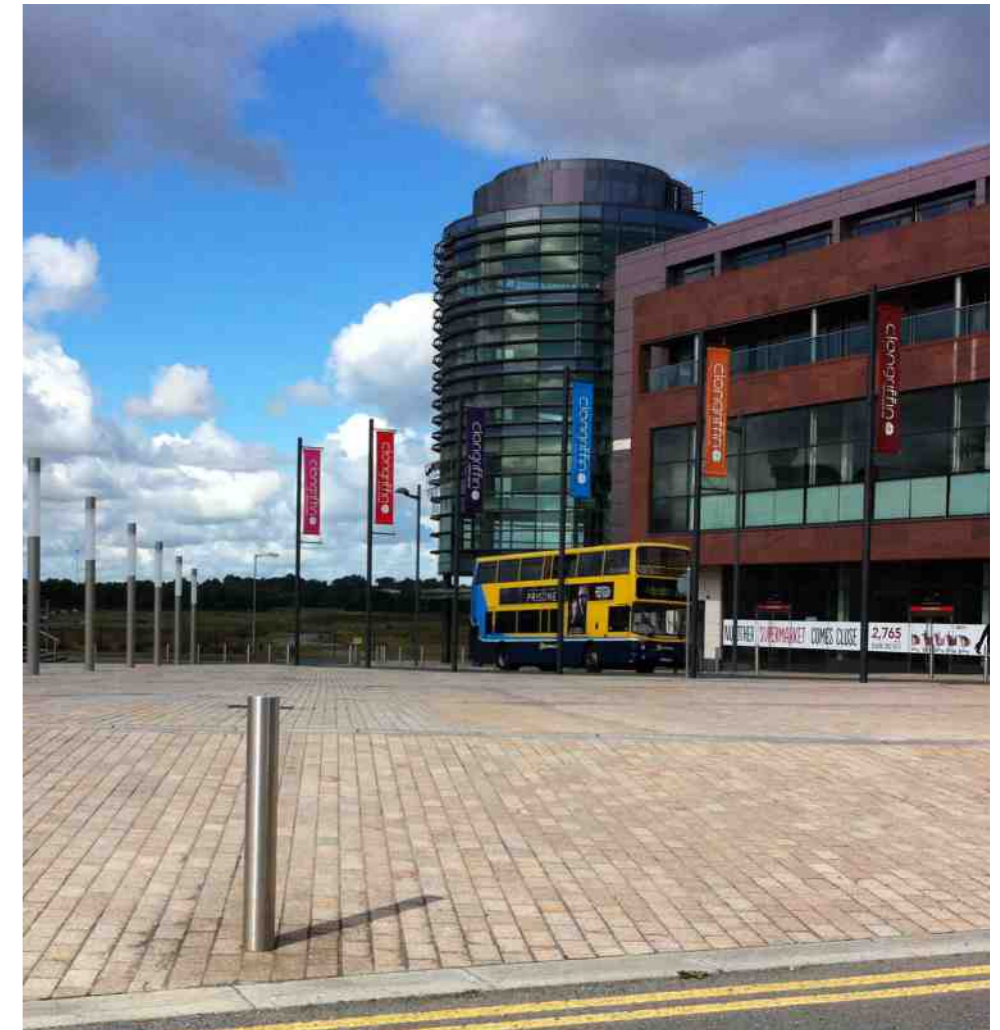
Street and neighbourhood names have been carefully selected to reflect the unique history of the area, including the former uses.

### Extract from 'Balgriffin Naming Report' by John Montague, Architectural Historian, 2005

3 North Donaghmede: E of Fr Collins Park, S of Mayne River, W of Dublin-Belfast train line

*This is the former location of Grange Abbey, the former grange to All Hallows (later Trinity College) Priory: a grange being the name for an outlying farm. The co-called Grange Abbey Church was also used as a local parish church throughout the middle ages, and its early-14th-century ruins still survive on a piece of open ground NE of the roundabout at the junction of the N32 and Grange Road. The names of Grange, Grange Abbey and Grangemore have been used already. Granted to All Hallows in the 12th century by the King of Leinster, Dermot McMurragh, Dermot's Grange is a viable new name. There is also a story from recent memory about the recovery of a bell which had for many years hung in a tree, but was found by the builders when the area around Newgrove Estate was developed in the 1970s. This bell belonged to the gable belfry of the original Grange Abbey church. The tree with the bell was a well known and mysterious feature in the area, before the tree was felled and the bell was recovered by the builders. The name Belltree gives some weight to recent history, while at the same time commemorates the very important medieval history of the area. Another piece of local and recent history was preserved until recently in the name given to the area now known as Fr Collins Park. This piece of ground had been referred to locally as Marr's Field in deference to the owner of the farm here (a Mr Marr from Howth). The new name Marrsfield is an acknowledgement of this local memory of an almost forgotten named place.*

Within the boundaries of the subject applications street have been named according the foregoing street naming convention and include reference to Belltree Park (Park Lane), Attenuation Lake (Lake Street) and town centre commercial uses including a weekly market (Market Street).

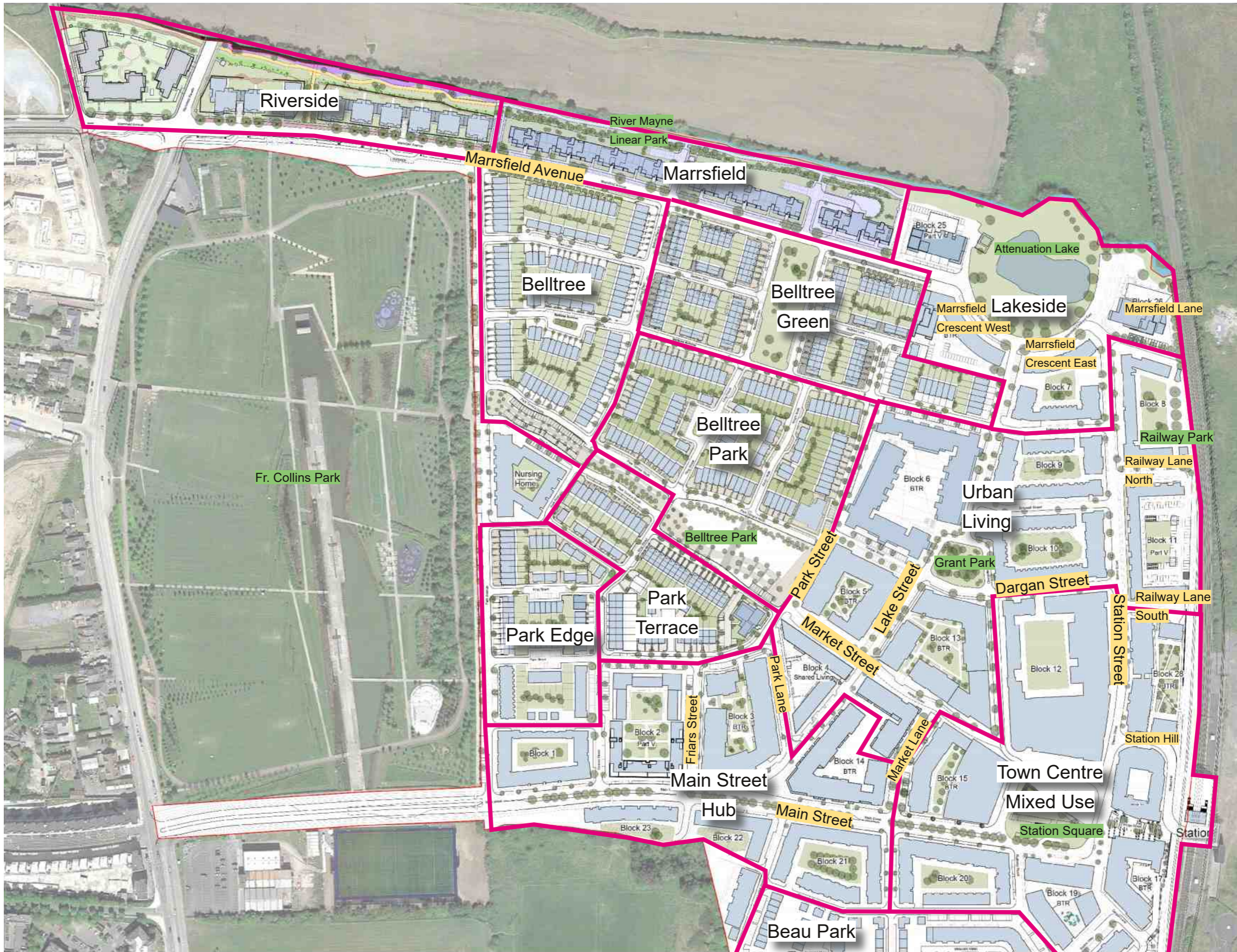


Clongriffin Station Square and commercial centre



Medium density single family houses along Park Avenue



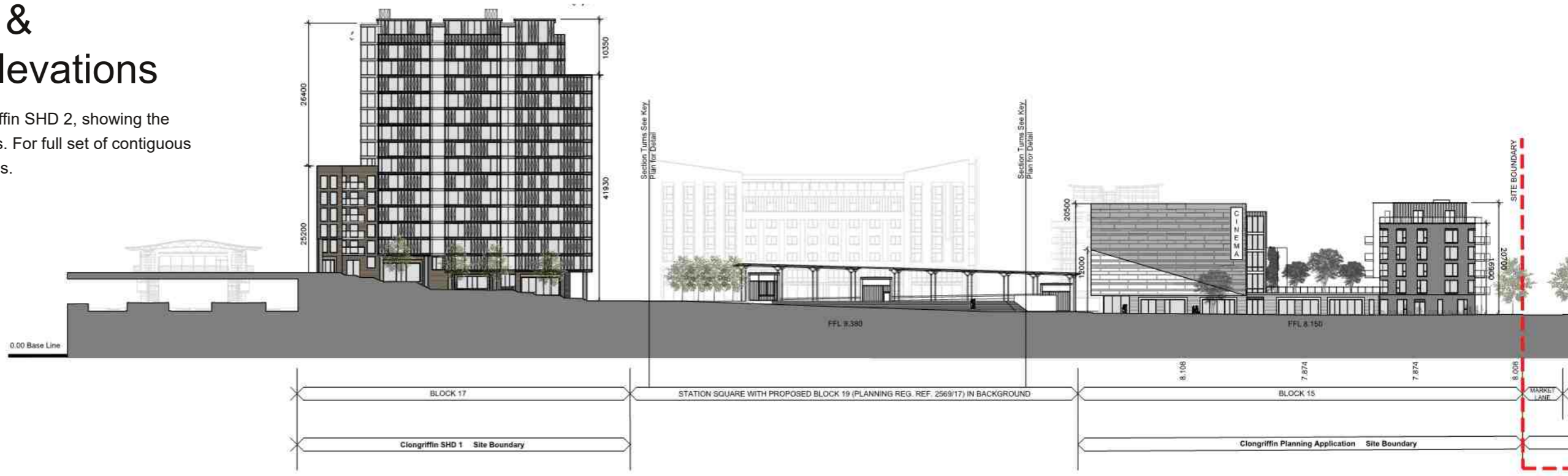


Neighbourhoods & Street Names

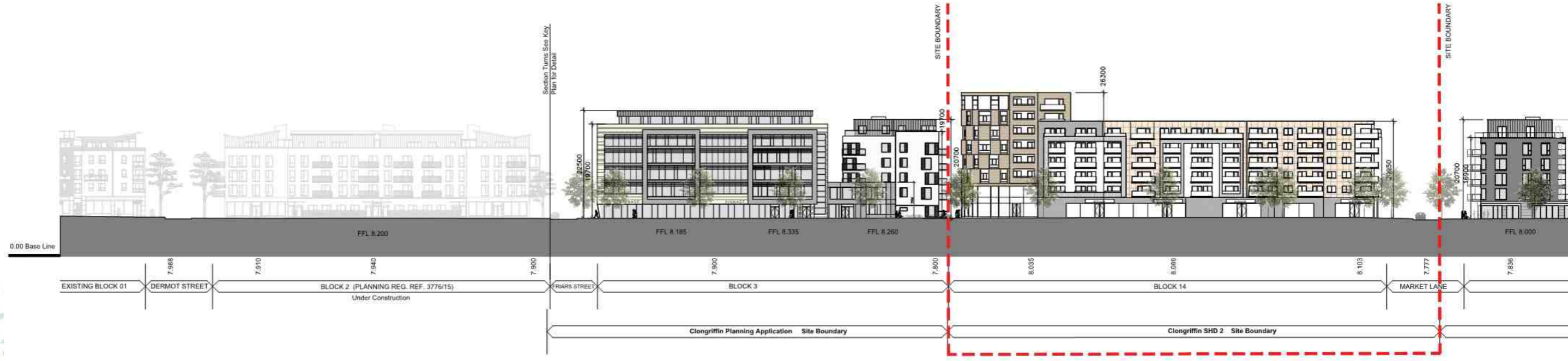


# 3.7 Site Sections & Contiguous Elevations

Sample contiguous elevations from Clongriffin SHD 2, showing the streets, context and concurrent applications. For full set of contiguous elevations please refer to full scale drawings.

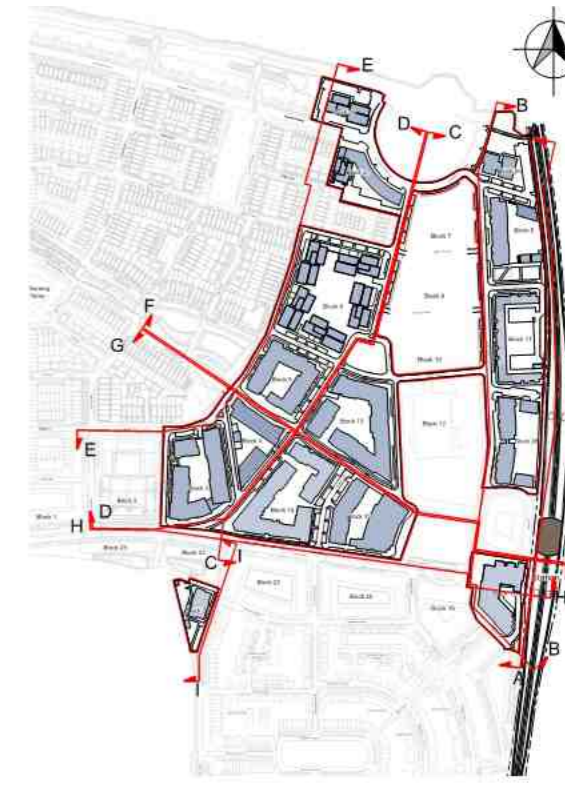


SECTION G-G

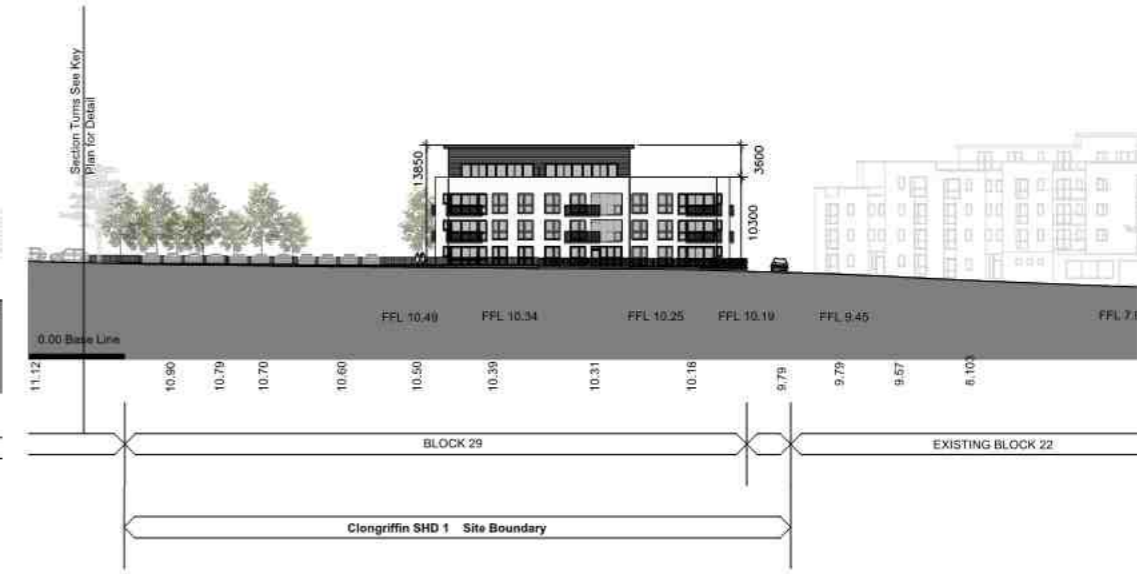
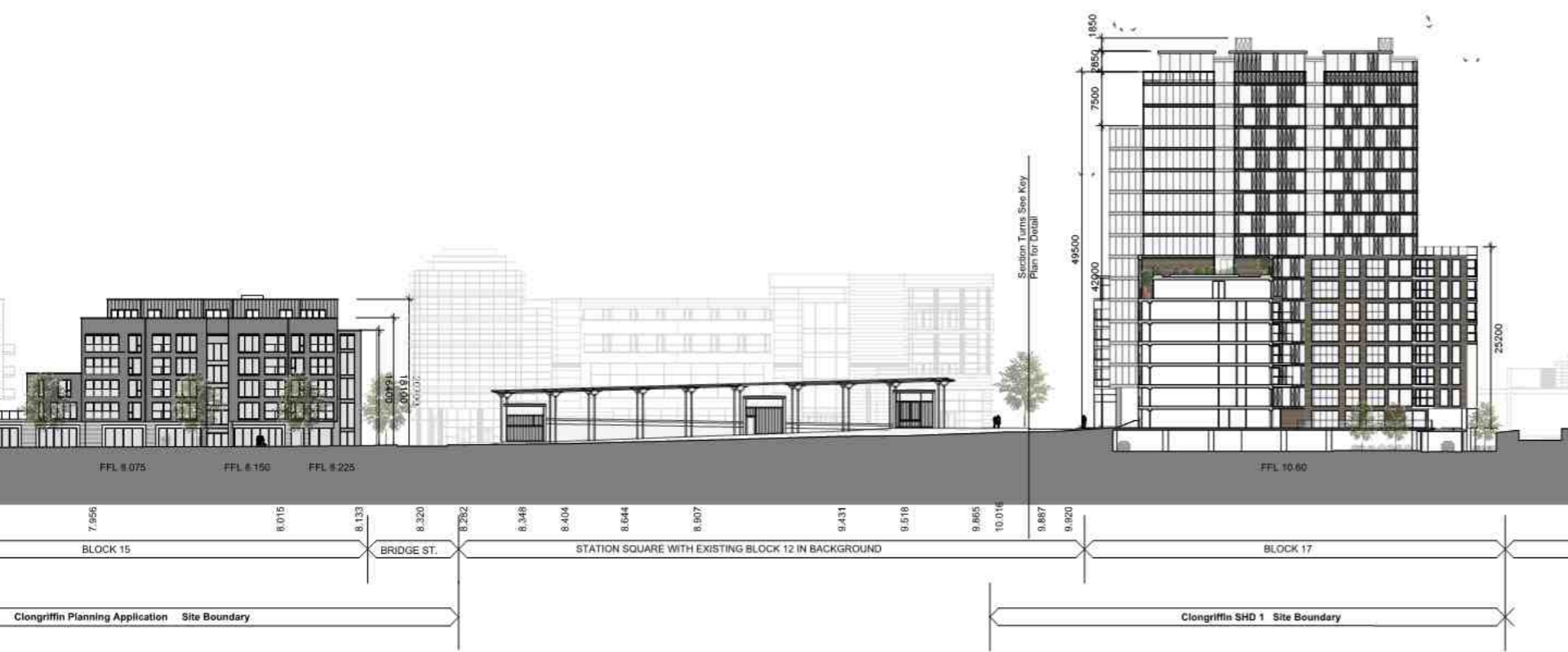


SECTION H-H





KEY PLAN



SECTION I-I



## 3.8 12 Urban Design Criteria

### Context

The subject application will integrate seamlessly with the context, both existing and emerging. The proposed blocks will slot into the street layout already defined in the Clongirffin Masterplan and largely complete on site. Each block will set up a strong street edge and aid wayfinding and legibility.

The proposals will integrate in terms of scale and massing, with blocks stepping up in scale from adjacent developments along Marrsfield Avenue and Main Street. This scale shift from housing to mixed-use has already been established, allowing the subject blocks to complete this pattern, culminating in high density and taller buildings adjacent to the commercial centre of Clongirffin.

The site is currently a brownfield vacant lot. Construction of these three new buildings will significantly enhance the context and help to complete this new town.

The material palette chosen will facilitate in knitting the proposals into the context, with similar choices of materials, a continuation of street and effective completion of the public realm. The blocks will also help to enclose Market Street and complete this important link to the station.

### Connections

Plentiful connections are proposed both within the subject application and to surrounding areas. The proposals will slot into a context of permeable streets, with priority for pedestrians and cyclists where possible. The form of the blocks is a result of desire lines and linkages in the wider area.

To the north the proposals front onto the River Mayne Linear Park, which connects both east and west, to the Baldoyle Nature Park and Belmayne respectively. To the south, the existing street network links through Beau Park to Donaghmede. To the west Main Street creates a strong physical and visual link back to Northern Cross and Belmayne.

Visual connectedness has been considered in the proposals, as this is an important way-finding and orienting device. Height, such as in Block 14, will aid with this visual connection, as well as generally straight roads including Market Street. A bend has been incorporated in Lake Street as a traffic calming tool, in line with DMURS, but this will not reduce the visual connections, as a result of the width of adjacent footpaths and incidental green space on the junction with Main Street.

### Inclusivity

All apartments within the proposed blocks have been designed to the latest standards and are extremely accessible for every user. All residents can access their core directly from the street. Alternative access routes are often provided, allowing residents options, including passing the concierge or entering through the car and bicycle parking area. A variety of apartments sizes will appeal to different people and requirements, including street level apartments, larger penthouse apartments, podium level apartments and social housing.

This will allow for a broad and mixed community within the proposed blocks as all needs can be accommodated.

### Variety

Blocks 4 and half of Block 14 are proposed as Build to Rent and offer a mix of BTR residential accommodation, including all associated amenity. These units should be suitable for a diverse mix of residents at all life stages. In addition there will be a small amount of commercial units provided at street level in all three blocks. This provides an ideal mix of uses to provide a diverse population travelling to and from the buildings.

The buildings vary in height and materiality, responding to their immediate context, this will add variety to the streetscape and the levels across the site. The proposed heights will vary from two storeys to eight. This variety and stepping of roofscape, in addition to occasional opening up of views into podium gardens or past blocks to the surrounding context, will allow glimpsed views of within and without the town and add interest to the streetscape and views for passers by.

### Efficiency

The proposed blocks make very efficient use of resources through high density residential development, at 200dph across the proposals. In addition, residential parking, bicycle stores and amenity space including landscaped podium courtyards and gardens, will also be provided. There will also be over 3,000sqm of commercial space provided.

This provides the best possible use for this brownfield site in an urban location, well connected to public transport.

### Distinctiveness

Each block subtly distinguishes itself from the surrounding blocks through form, including different heights, from two to eight storeys, different roofs from flat to mono-pitched, different forms, from perimeter blocks to pencil blocks and different material palette, from brick to curtain walling. Other devices employed to distinguish each block include Block 4, opening up to the west with the lower scale along Park Lane, Block 14 opening up along its frontage with a glimpsed view into the internal garden and Block 5 with its office frontage.

These distinctions, along with other details outlined in each individual design report, will add diversity and interest to the streetscape, aiding wayfinding and legibility.

### Layout

The proposals are laid out to make the most of this site with ground floor retail and residential units extending to the back of pavement and high density residential uses surrounding and above. The podium courtyards are typically oriented to receive maximum south and east light, enhancing amenity value. Active frontages will focus activity on the street and provide passive surveillance. These include concierge entrances, retail units where appropriate and own-door residential uses.

The street layout is legible and permeable. The blocks are laid out to provide screening and a buffer to the railway line to the east, frontage onto the River Mayne Linear Park to the north and a strong street edge along all streets within the town centre.

### Public Realm

The proposals locate parking spaces on-street to add activity to the public realm. Tree planting and public lighting are also proposed. These will all be finished with the highest quality materials, creating a high quality public realm. Street parking will incorporate SUDS measures through permeable paving. Parking, in addition to active frontages will aid amenity and activity on the street.

### Adaptability

Ground floor units can prove highly suitable for residents with additional mobility or accessibility concerns. Ground floor and podium level units have been designed with adaptability and own-door access in mind. As all the blocks will be centrally managed by an institutional body, be it a social housing body or private BTR or BTS management company, it may be possible for residents to stay within their block, or the town, and move to a different unit, if their circumstances change and they need more/less space or have other new requirements.

### Privacy and Amenity

All apartments are designed with privacy and amenity in mind. Overlooking has been kept to a minimum between apartments. Balconies will be screened.

### Parking

Car parking has been provided at a reduced rate of 0.7 spaces per unit across all the blocks in the application. This is in line with relevant guidelines and experience to date in Clongirffin, with earlier phases. Parking is provided communally in podium and basement car parks and immediately surrounding each block in on-street spaces. A separate parking report is submitted as part of this application. A separate parking report is submitted as part of this application by Moylan Consulting Engineers.

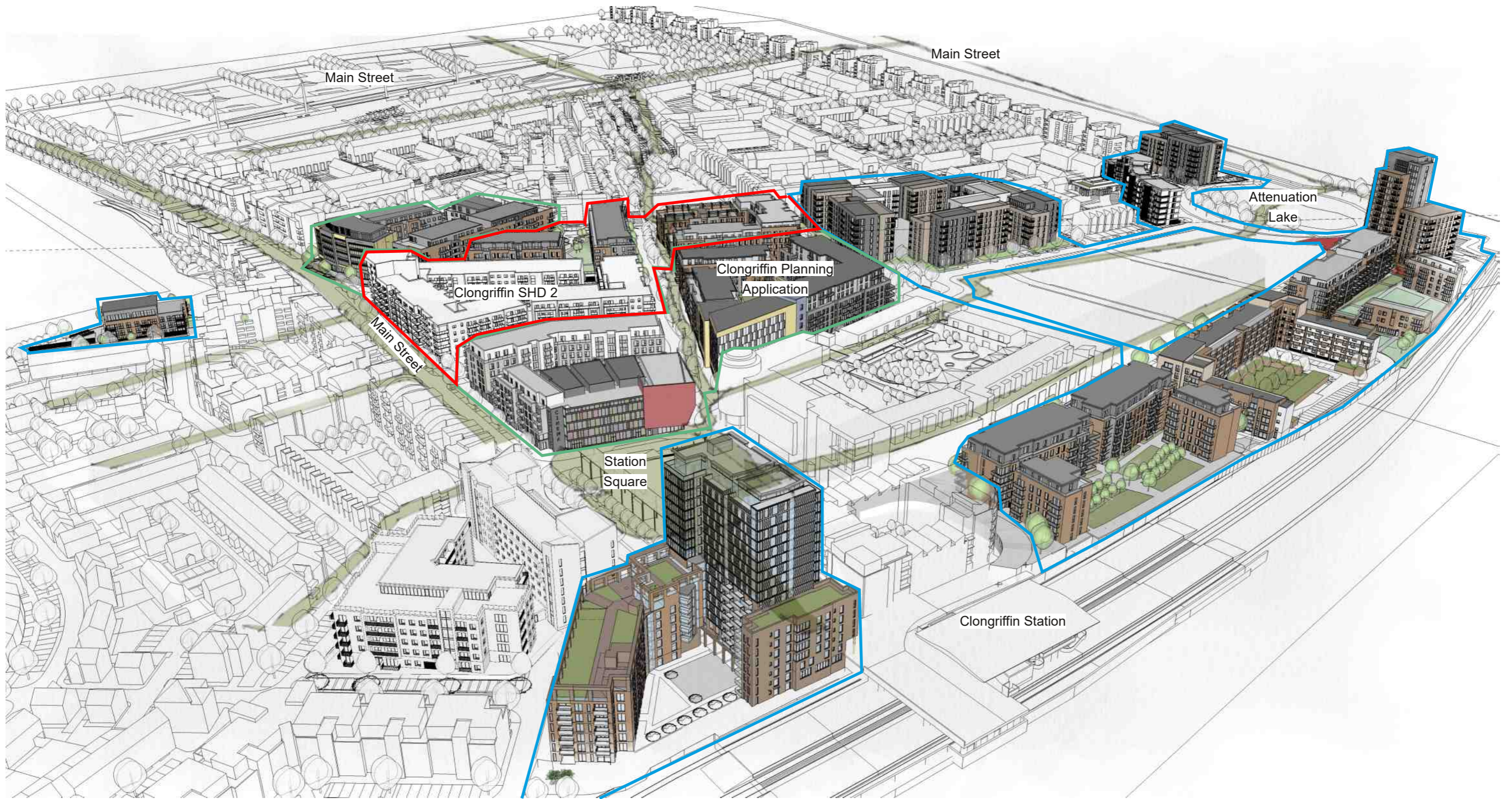
Bicycle parking has been provided at a rate of 2 spaces per apartment and meets all relevant guidelines. Further parking and car share vehicles will be available for residents within the large car park at Block 12.

### Detailed Design

Each block will be constructed with a complimentary palette of materials to its neighbours. This will aid cohesion in Clongirffin without resulting in sameness and repetition. The materials have been chosen for their durability and weather proofing. Public realm will be constructed with one palette of materials and planting to draw the whole Town Centre together. Access, levels and amenity have all been carefully considered in the detailed design process.

Further detail can be found in each individual design report and the scaled drawing set that make up this application.







## 4.0 BUILDING DESIGN

### 4.1 Design Overview

There are 500 apartments proposed in this application. Of these, 265 are Build to Rent (BTR). The advantages and distinctions of this rental model has been outlined earlier in this report.

In each of the BTR blocks residents can enter their own lift and stair core directly from the street or car park, with alternative options typically including access via a shared concierge or through a landscaped podium courtyard. Resident support facilities generally include a concierge, flexible amenity rooms and meeting rooms, and all the required services such as bicycle stores, bin storage and plant rooms.

A total of over 949.5m<sup>2</sup> of internal residential amenity space is provided across the two BTR blocks proposed.

A further 138 apartments are proposed as Build to Sell (BTS). This will add diversity and mix to tenure within the town centre. Build to sell blocks still benefit from residential amenity rooms where feasible, including concierges and amenity rooms.

97 apartments are proposed as social housing and these are designed to the same high standards, including high quality landscaped gardens and private balconies, open plan living areas and large bedrooms.

All apartments, including BTR, BTS and social housing, are designed to the latest standards outlined in Sustainable Urban Housing: Design Standards for New Apartments (March 2018). All apartments include open plan kitchen/ living/ dining areas and private balconies and roof terraces. Living spaces are generously designed with aspect maximised where possible and large windows providing lots of light.

The Blocks are each tailored to their place within the development with all providing strong street edges, landmarks, mixed-use and passive surveillance.



View of Block 14 from Main Street



## 4.2 Block Layout & Massing

The subject application includes three perimeter blocks. Each block responds to its immediate context and is laid out to maximise the site and location it finds itself in.

Block 5 bridges between the residential scale to the west and the higher density and height of the town centre, with three storeys to Park Street and seven to Lake Street. Block 4 provides a community hub for the town through its proposed uses and central location. Block 14 provides a landmark on the junction of Main Street and Lake Street, which will help with legibility and wayfinding in the town. All three blocks provide active frontages and enclosure to Market Street.

Perimeter blocks provide efficient use of plots, with full site coverage through podium level parking in Blocks 4 and 5, with landscaped courtyard above. Block 14 has a similar layout, with a basement car park and garden above. This allows the building line to move to the back of pavement, helping to enclose streets and provide structure to the public realm.

The roofscape of the proposals will be dynamic, with different building heights and finishes. Set-back penthouse levels, flat roofs with overhangs, glimpsed views into the semi-private gardens and sharp edges all working together to create interest.

At street level, shopfronts are identified by use of material changes and glazing. Above, full height windows allow lots of light into the apartments and provide a vertical structure to the elevation. The changes in roof height adding to this verticality.





# 4.3 Material Palette & Elevations

## Block 5; Build to Rent Example

The building finishes are designed to be robust, durable and maintenance free. Brick is chosen as the primary material for the residential accommodation over levels one to four. A mixture of bricks is employed to add interest and break down the scale and grain of the blocks.

The residential facades are pushed and pulled over their length and height with balconies, recessed roof terrace and articulated entrances to residential cores. A light zinc or similar cladding will be used at the recessed penthouse level. Flat roofs will be single ply membrane or extensive sedum green roofs as indicated on roof plans.

A painted steel pergola structure supports the balconies and opaque glass privacy screens on the west facade. This structure will also support a cable green wall system at the terminal point of Bintree Park which is currently under construction. A similar pergola structure at the 4th floor roof garden on the west wing of Block 5 will continue the theme from the facade.

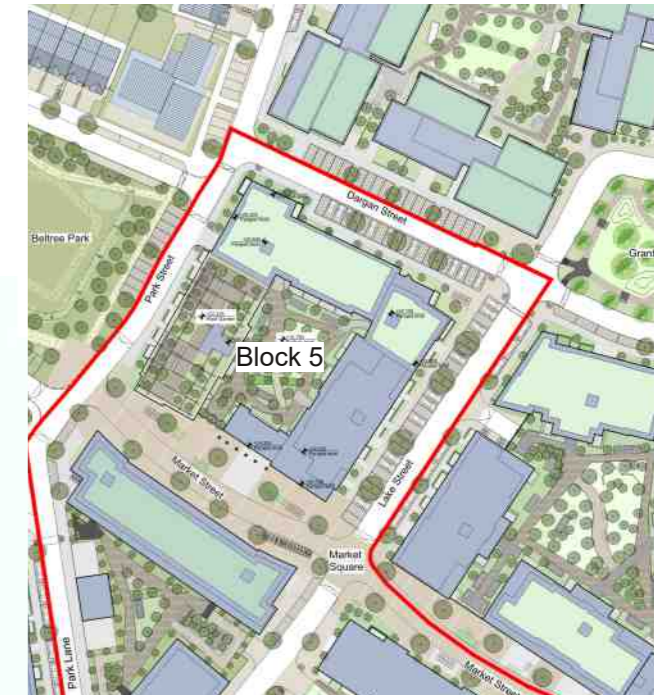


Key precedent images of material palette for Block 5



BLOCK 5 - WEST ELEVATION TO PARK STREET





**Wind Screens**

1.8m high wind screens in transparent glass are provided to the ends of balconies adjacent to corners of the building which were highlighted by the wind report.

**Brickwork facade**

The primary facade cladding palette is a mixture of Natural red bricks and Dark grey bricks which create a variation in the facade of the block and break down the scale and grain of the blocks.

**Steel and Glass Balconies**

Steel and glass is the primary materials used for balconies and balustrades. Translucent glass is used on certain balconies to provide more privacy.

**Painted Steel Pergola**

Provides structural support to balconies on this facade as well as accommodating opaque privacy screening between adjacent balconies and green wall cable system



# 4.4 Apartment Design

## Block 4; Build To Rent Example

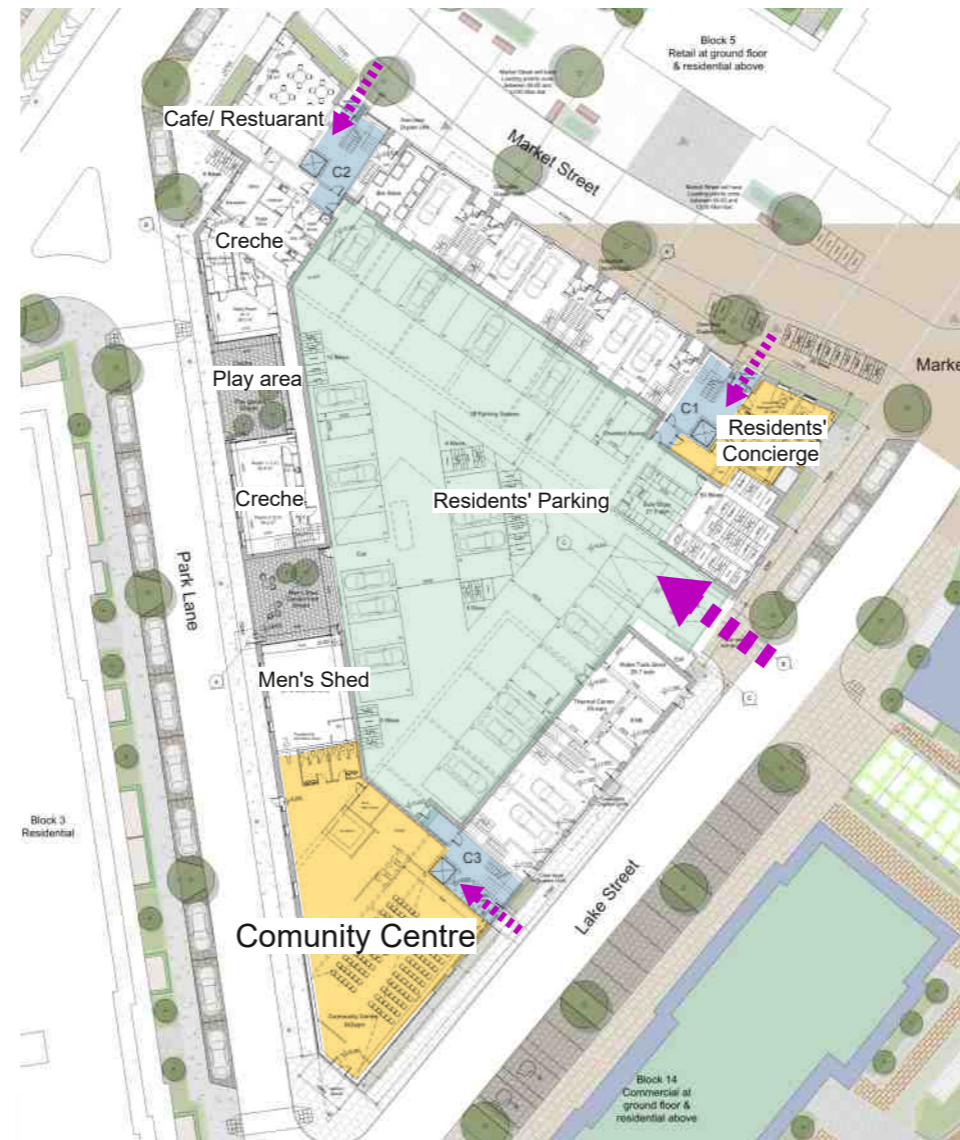
Full detail of the apartment design for each block can be found in each individual Architect's Report. Sample spreads are provided here for reference.

The apartment layouts are based on a build to rent model. Residents can enter their own lift and stair core directly from the street or podium parking if they wish. There is also a shared access point at the east corner, prominently located on Lake Street. Resident facilities are located at ground and first floors and include a concierge and amenity room. The journey through the resident access on Lake Street is detailed on the right.

Apartments are designed to the latest standards outlined in Sustainable Urban Housing: Design Standards for New Apartments (March 2018). All apartments include open plan kitchen/ living/ dining areas and private balconies and roof terraces. Living spaces are generously designed with aspect maximised where possible and large windows providing lots of light.

The building is laid out to encourage community amongst residents, through the provision of high quality communal facilities. Research in the field indicates that residents are more likely to feel a sense of 'home' and 'community' if they know even a small number of their neighbours.

Both internal and external communal facilities, in addition to a shared access point through the concierge, allow residents ample opportunities for spontaneous meetings and exchanges. The concierge provides additional amenities as residents can have parcels and deliveries securely received during the day.



Sample core



# 4.5 Build To Rent

Within Clongriffin Planning Application Blocks 4 and half of Block 14 are proposed as Build To Rent (BTR), representing more than half the units proposed. As a result of this large proportion of BTR, which is relatively new in the Irish market, the design team have looked to international examples of best practice for guidance.

Each block has a range of bespoke amenities included within. These include bulk storage rooms, where residents can store furniture or other oversized items outside of their apartments, meeting rooms which will be available to hire, laundry rooms, concierge services, resident gyms and large entertaining rooms.

Where relevant, amenity rooms are provided with optimum aspect, wc facilities, level access, privacy, secure key systems etc. All amenity and support facilities will be maintained and managed by the management company.

A report, which examines and provides a justification for the scale of proposed Build to Rent tenure within the proposed and concurrent applications, having regard to the creation of sustainable neighbourhoods within the context of Clongriffin, has been produced. This report examines the provision, both quantitative and qualitative, of residential support amenity areas, within the overall scheme and on a block by block basis, having regard for both Section 5 and SPPR 7 & SPPR 8 of Sustainable Urban Housing: Design Standards for New Apartments, Guidelines for Planning Authorities (March 2018). That report has been submitted as part of this application.

All amenities provided comply fully with the guidance set out in Sustainable Urban Housing: Design Standards for New Apartments (March 2018). An average of 2.9m<sup>2</sup> is provided per unit through all concurrent applications, with 3.7sqm avg in SHD 2. Typically, exemptions are not being sought for reduction in standards of apartment storage space, private outdoor space, units per core, mix etc.

Detail of BTR support facilities and amenity can be found in each individual Architect's Report.

Block	Support Facilities (sqm)	Detail	Services & Amenity (sqm)	Detail	External Amenity Space (sqm)	TOTAL Internal (sqm)	Apartments	Internal sqm per unit
Block 4	71	Manager's office, guest suite, laundry, bulk storage	133.5	Residential amenity room, study room, games room	1,002	204.5	74	2.8
Block 14B	295	Concierge	450	Amenity room, bulk storage, games room	1,977	745	191	3.9
TOTAL	366		583.5		2,976	949.5	265	3.6



Images of sample build to rent facilities from other schemes, including concierge, residents' gym, games and entertaining room, residents' laundry and small meeting room.



## 4.6 Building Heights & Adjacencies

In accordance with the LAP objectives, building heights have been employed to reinforce street hierarchies, aid wayfinding and structure public spaces.

The proposed blocks vary in height from two storeys to eight. Typically, building heights along the primary streets are six storeys, with the most height located at the important node of Main Street and Lake Street.

The west side of Block 4, at two storeys, is the lowest height in the proposals. This is reflective of the uses proposed in this block, in addition to the constraints of the plot restricting the ability to enclose the block without overlooking. Community and creche uses are located along this side of Block 4.

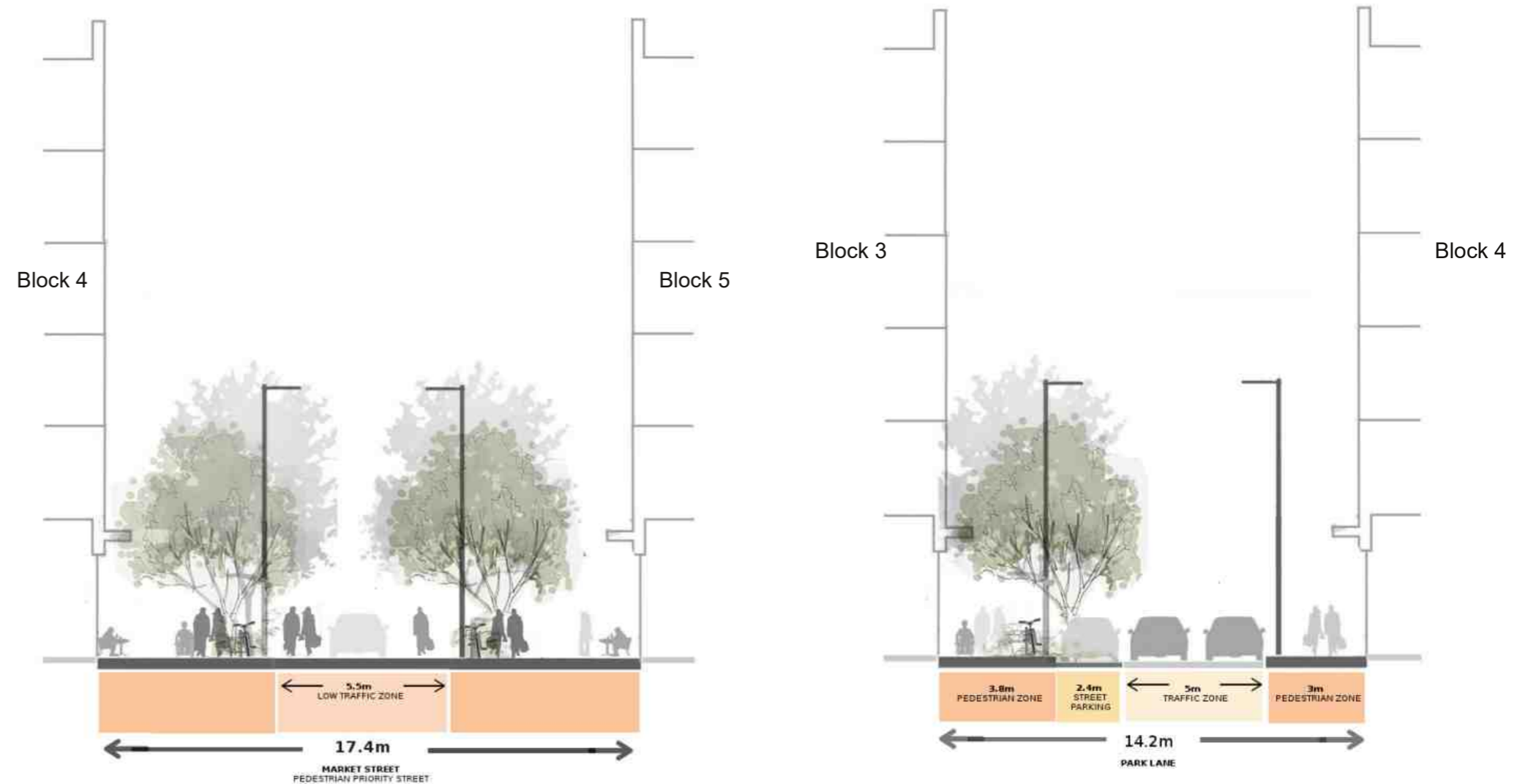
At a maximum of eight storeys, Block 14 has the most height. This height is located locally on a prominent corner and is a structuring device, for identifying this key node in the street network.

Streets are wide at approximately 16-20m between building faces in many areas. This is reflective of the high quality street environment being proposed. This includes generous footpaths of at least 2m on each side of the road, on-street parking bays 3m deep (2.4m wide plus 750mm door swing buffer), carriageways are 5.5-7.8m wide. This arrangements prioritises pedestrian movement, with more space allocated to footpaths, tree planting and street parking than traffic.

The sections show the horizontal arrangement of space on both Market Street and Park Lane.

The first is taken through Market Street and shows the division of space horizontally. The street will be well enclosed by buildings on each side, with a generous width of 17m+. Tree planting, shared surfaces, benches and active frontages on both sides will activate the street and create a strong pedestrian and cyclist priority. Proportionally, the width and breadth of the street will be similar, which is a very comfortable scale and environment, with plentiful light penetration. A carriageway of 5.5m will be indicated through material colour change only, with no kerbs nor level change. This will encourage pedestrians and cyclists to occupy this space, whilst also allowing more nervous or partially sighted users to clearly understand where this boundary is. A dark band will indicate this boundary with high contrast.

The second section is taken through Park Lane. This will be a local access street and is narrower as a result. Generous footpaths, combined with on-street parking, will combine to slow traffic and create a neighbourhood feel.







Block 4 massing study

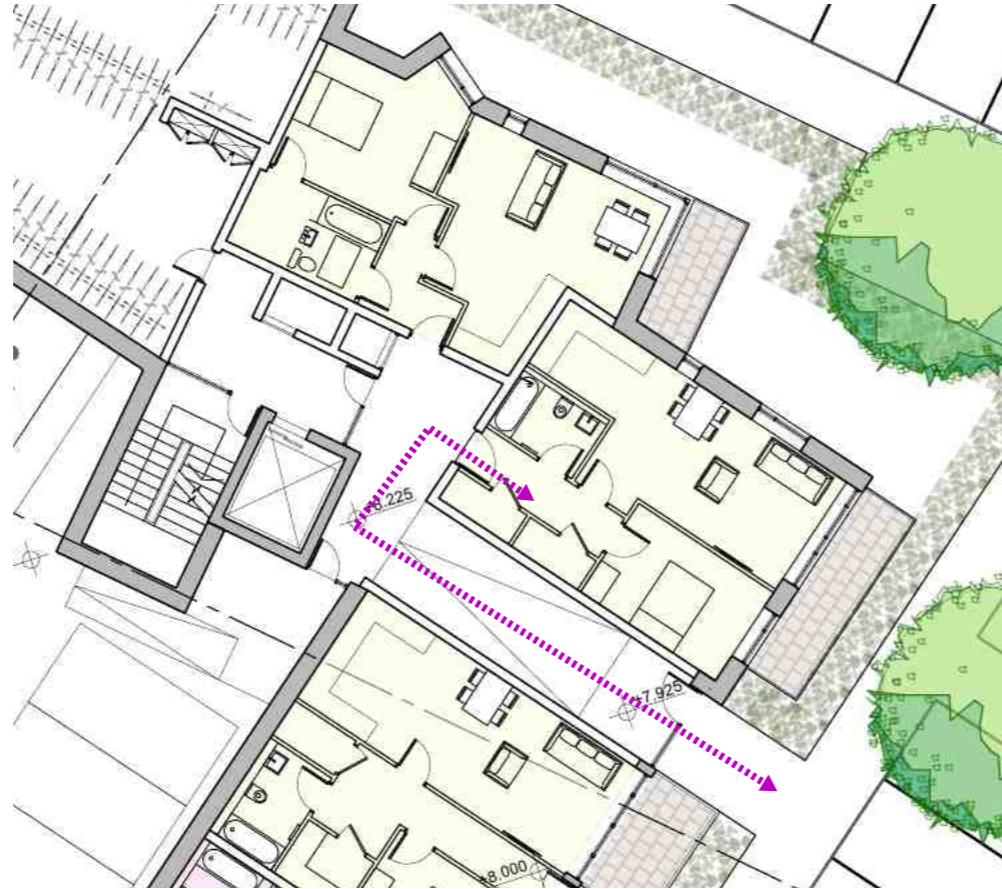


# 4.7 Threshold Studies; Ground Floor Units

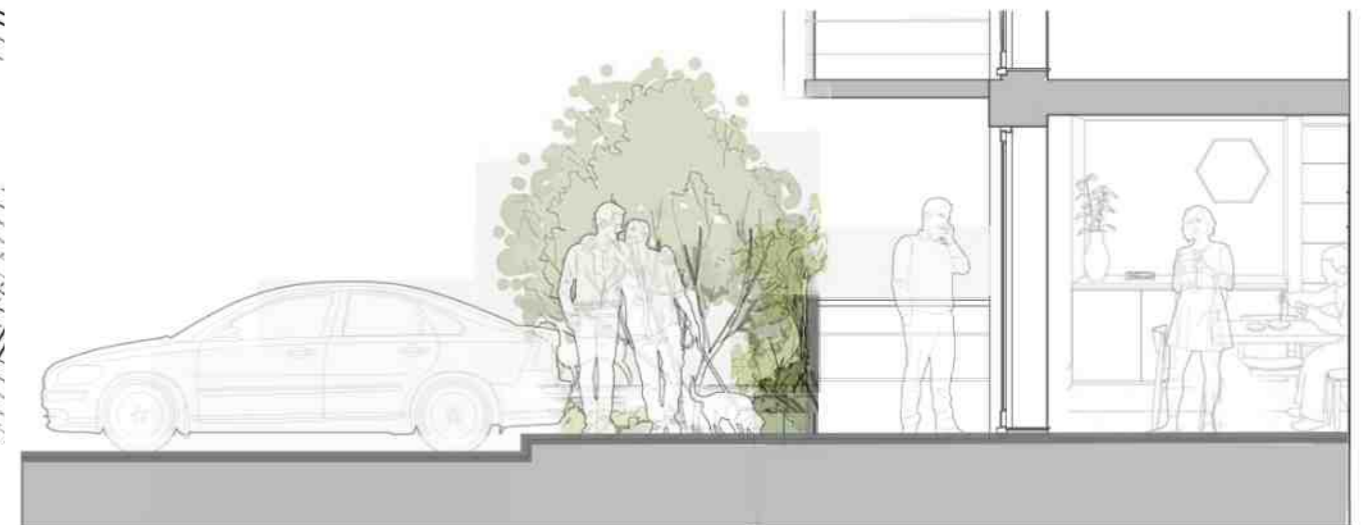
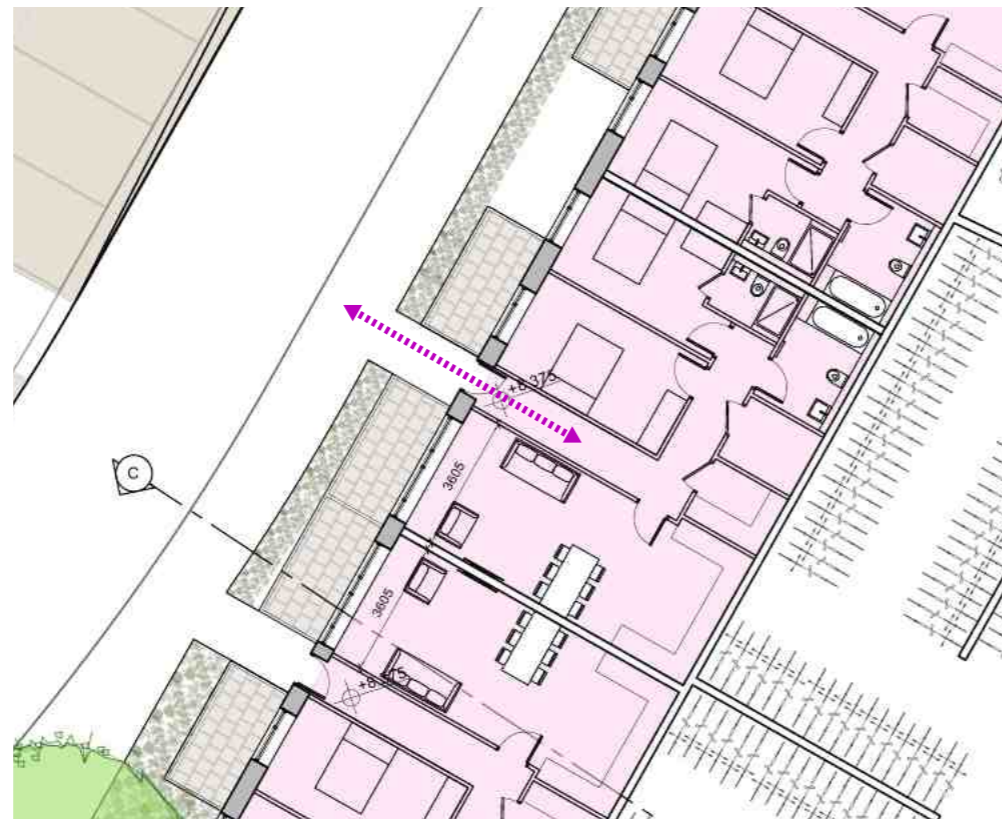
Where ground floor units with private open space onto the footpath occur there are two approaches to the entrance and threshold. A combination of these approaches is taken in Block 5 to add diversity and options for residents.

Option one raises the threshold of private open space above the public footpath. In this scenario the apartment entrance is located within the building, from the communal circulation area. Residents are raised above the public street when on their patio. With careful planting the patio can be quite private, especially whilst seated. This arrangement still allows 'eyes on the street' as a result of the proximity of residents to the street, but it also creates quite private living and open spaces with little likelihood of passers by peeping into apartments.

Option two creates small patio spaces with apartment entrance from the street. The advantage of this arrangement is that it increases active frontage with residents coming and going from their apartments directly onto the street. As residents will enter through their private open space they may feel inclined to add plants and furniture and to use this space. Hedges and other planting can still be carefully employed to create privacy, especially whilst seated. The main advantage of this scenario is increased activity and passive surveillance from residents coming and going from their front doors.



Option 1; raised threshold, entrance from behind



Option 2; flush threshold, entrance from street



## 4.8 Wind Mitigation

A wind impact assessment was undertaken by Arup Engineering. All recommendations have been incorporated into the block designs. Please refer to Arup report for further detail.

### Wind Mitigation Block Summaries

Block 3  
No mitigation required - sheltered location

Block 4  
No mitigation required - sheltered location

Block 5  
At the roof garden, a 1.8m high screen was incorporated at the south end to provide added comfort to residents at this exposed wind location. Wind screens 1.8m high were provided to some balconies along the north and west sides, particularly adjacent to high speed with corners due to west and south west winds. Landscaping was intensified along the Park Street side and pedestrian footpaths pushed as far away as possible from the high speed wind corners of buildings.

Block 6  
This Block has had several interventions to mitigate the impact of wind. On street level, landscape has been intensified in areas deemed to have a risk of high wind speeds, notable the corner of Dargan Street and Park Street and also the corner of Park Street and Belltree Avenue. On podium level, the gaps found between the perimeter blocks have also seen an increase in landscaping due to the possibility that wind could be funneled between them. Many balconies with two sides exposed, particularly those facing the street, now include full height wind screens on their short edge. All roof terrace parapets have been increased in height from 1200mm to 1400mm to further mitigate the wind.

Block 8  
Wind funnelling was found to be an issue on the minor streets to the north and south of Block 8 (Marrsfield Lane and Railway Lane North) which terminate to the open expanse of the railway line. Landscape planting was intensified at the green edge along the eastern site boundary as a result. This had a knock on effect where there were gaps within Block 8 at the north and south ends of the podium. A combination of 1.8m high wind screens and intensified landscape planting was employed to mitigate the effects for residents. In addition a number of the balconies were highlighted as needing 1.8m high wind screens on their ends so the design was revised to accommodate these.

Block 11  
The same funneling issue at the minor streets to the north and south arose at Block 11 so 1.8m high wind screens were introduced at some of the balconies to Railway Lane North and Railway Lane South. Landscape planting was intensified at the green edge along the eastern site boundary at the end of these street also.

Block 13  
Balconies adjacent to corners to the north and east elevations were provided with 1.8m high wind screens either side. Wrap around balconies were removed due to their high exposure to wind.

Block 15  
Balconies and podium were relatively sheltered. Landscaping introduced at south east corner to provide protection for pedestrians at this high speed wind corner.

Block 17  
The main entrance passage from Station Square was found to be liable to wind funnelling so the proposed pedestrian gate at the west elevation was mirrored at the east end. The corner of Bridge Street and Station Square was found to be vulnerable to downdraft wind effects so a wrap around canopy has been provided at second floor level to mitigate the effect of this for pedestrians. On the south side of the tower element a high level canopy was provided at 8th floor level to protect pedestrians below from downdraft.

Block 28  
Wind funelling and high speed corners were found to be issues along the north and south of Block 28 with particular effect on the gaps between the blocks. Landscaping and wind screen to the north in conjunction with landscaping and the proposed pedestrian gate and fence to the south were employed to mitigate against this. 1.8m high wind screens were provided to the sides of balconies in these locations also.

Block 29  
The balconies adjacent to the high speed wind corners were modified so that they did not wrap around the corners and 1.8m high wind screens were provided to either side of these balconies to provide protection for residents.

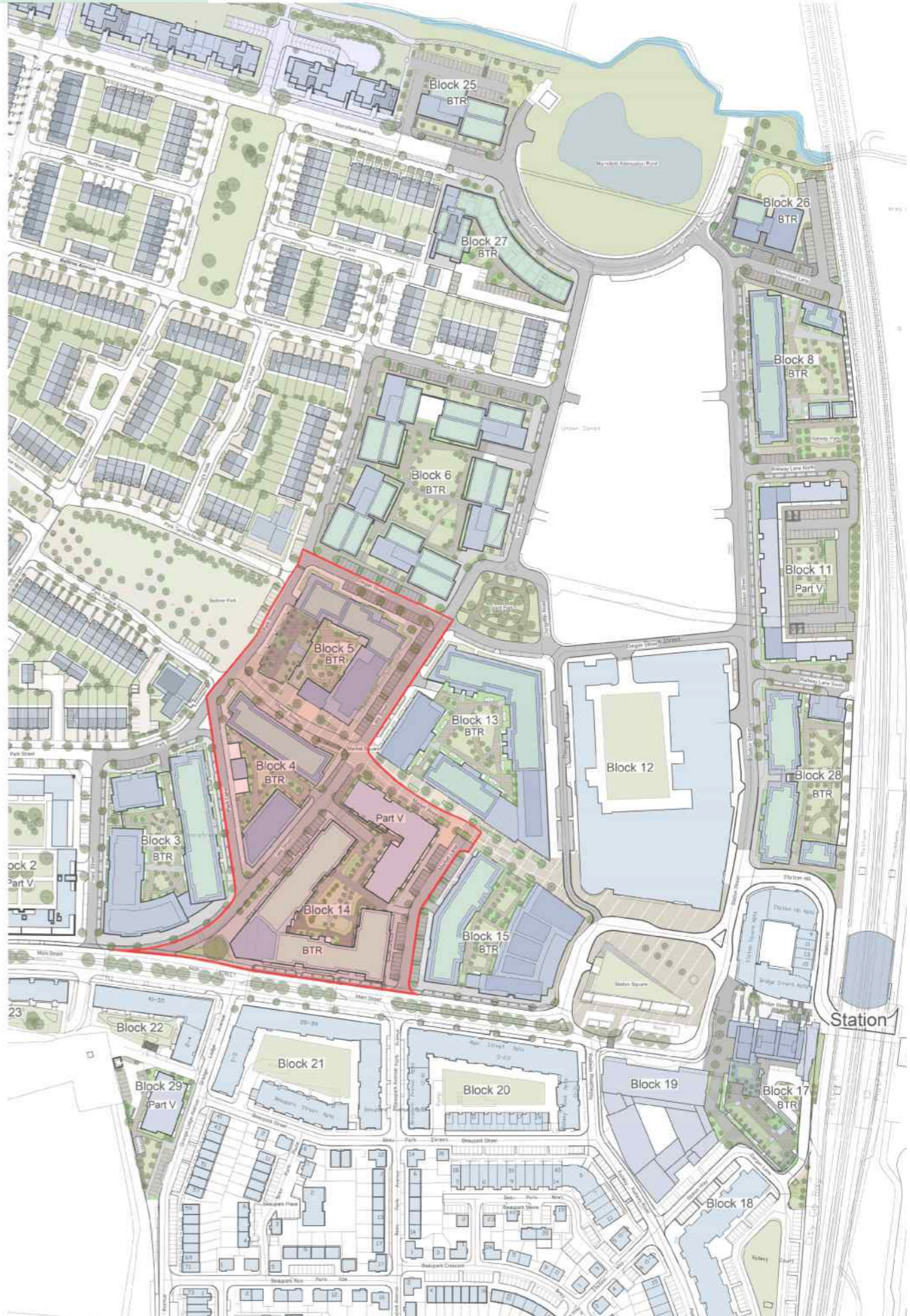
Block 25  
This blocks balconies on the corners now have full height wind screens on their short edge and the roof terraces parapet have been increased in height from 1200mm to 1400mm.

Block 26  
This blocks height has raised several concerns regarding wind speeds. In order to combat this, landscaping has been increased at the foot of the building and several canopies are proposed to mitigate downdrafts. Wind screens are also proposed along the short edge of the balconies at the outside corners of the block. A canopy has been added to the 8th story roof terrace and both roof terraces parapets have been increased from 1200mm to 1400mm.

Block 27  
The risk from wind at this block is minimal and the only mitigation required is full height wind screens on the short edge of the balconies found on the eastern end of the block.



# 5.0 BLOCK PROPOSALS OVERVIEW



Masterplan site layout showing subject sites - Blocks 4, 5 and 14



# 5.1 Block 4 - Overview



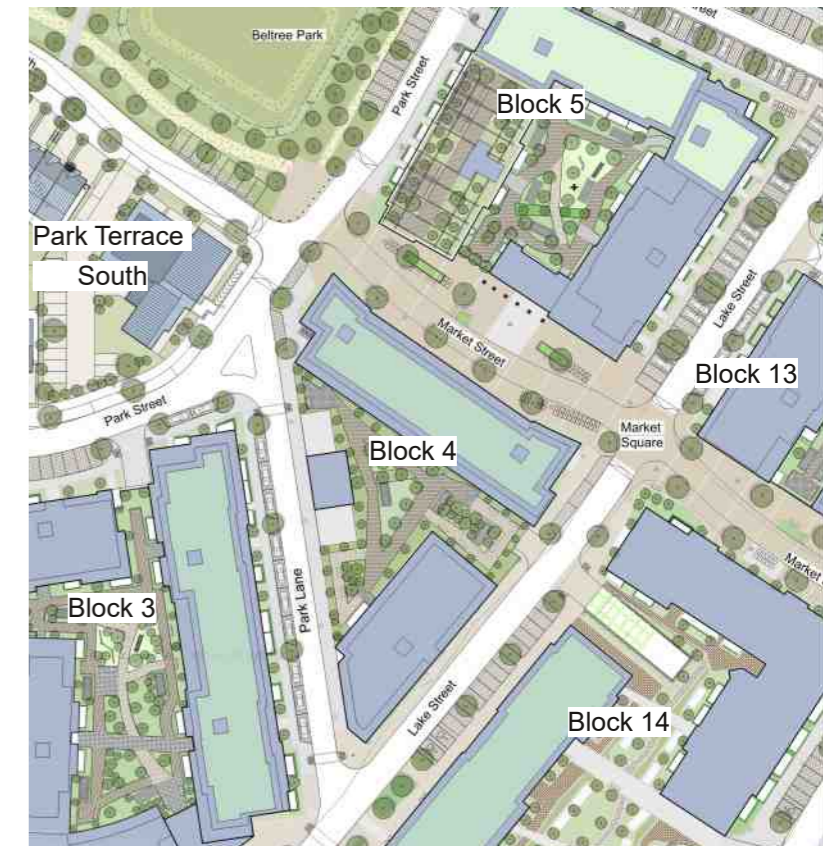
Block 4 is a 0.513 hectare site located at the heart of Clongriffin enclosed by Lake Street, Market Street and Park Lane. The unique mix of uses proposed within the block will make it a hub for the community. 74 Build to Rent units are proposed above a creche, café/restaurant unit, community centre and Mens' Shed at ground floor level.

At ground floor, residential amenity including a concierge and at first floor a residents' amenity room. A prominent Community Centre entrance to the south, in addition to significant frontage along all three surrounding streets will activate the streetscape around Block 4. The restaurant/ café unit is located on corner of Market Street and Park Lane, Market Street is a pedestrian and cycle priority street linking Fr. Collins Park to Clongriffin Station Square.

The proposed residential mix is 9no. 1 bed units, 55no. 2 bed units and 10no. 3 bed units. This generates a mix of 12% 1 beds, 74% 2 beds and 14% 3 beds.

The proposed commercial offer includes a café/ restaurant unit, a creche for 53 children, a community centre and a Mens' Shed.

Internal residential amenity space is provided at both ground and first floor level and combines to 204.5m<sup>2</sup> in size.



Block 4 west;  
2 storeys of creche and Mens' Shed along Park Lane, including private open space for the creche

Block 4 east;  
5 floors + penthouse of BTR residential units above Community Centre and concierge.



View of Block 4 on Lake Street approach

Site Summary: Build to Rent		
Total Residential Units	74 nr. (277 bed spaces)	
Total Commercial Area	799sqm (gross)	
Site Area	0.513 ha (1.26 acres)	
Plot Ratio	2.03	
Site Coverage	95%	
Net Density	144 units/ha (59 units/acre)	
Building Height	5 storeys + penthouse (residential) 2 storeys (creche)	
External Amenity Space (Podium)	1,002 sqm	
Internal Amenity Space (BTR)	204.5 sqm	
Parking	45 spaces off-street, under podium 5 spaces on-street 50 Total	
Cycle Parking	132 spaces	
Schedule of Accommodation		
1 Bed Units	9	(avg. 56sqm each)
2 Bed Units	55	(avg. 88sqm each)
3 Bed Units	10	(avg. 105sqm each)
<b>Total</b>	<b>74</b>	
Cafe/ Restaurant	78 sqm (gross)	
Creche	304 sqm	
<b>Community (incl. Mens' Shed)</b>	<b>417 sqm (gross)</b>	
<b>Total</b>	<b>799 sqm (gross)</b>	



# 5.2 Block 5 - Overview



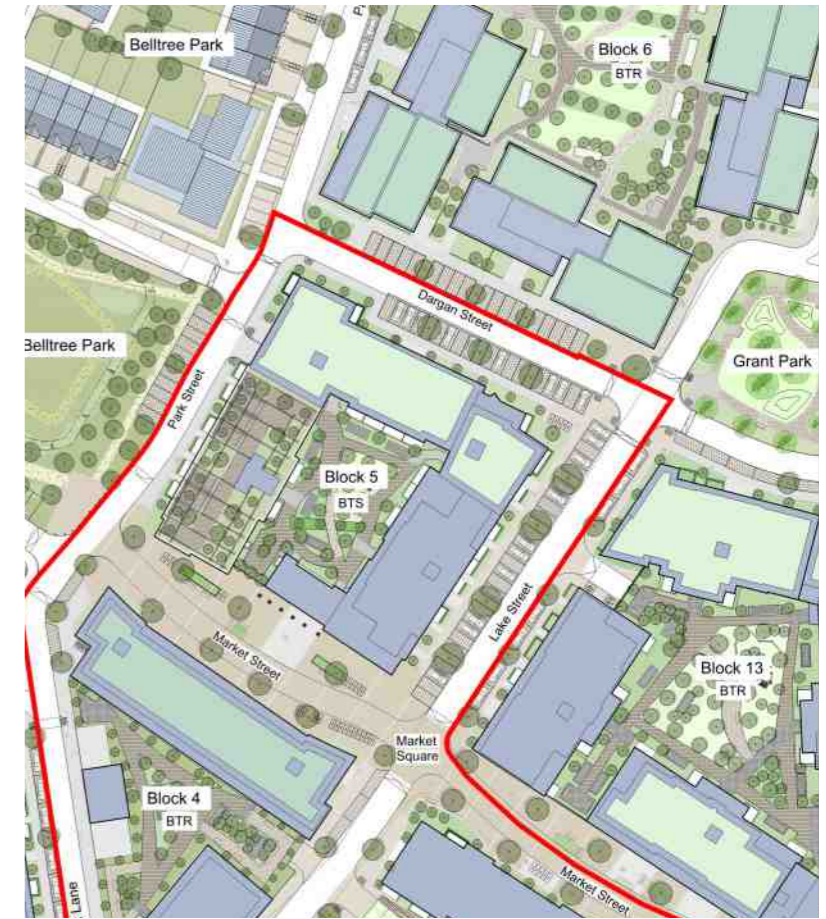
Block 5 is a 0.70 hectare site located at the heart of Clongriffin enclosed by Park Street, Park Lane, Dargan Street and Market Street. The block is made up of a mix of commercial and residential uses with 138 apartments and 393m<sup>2</sup> of retail space.

Block 5 is proposed to be Build to Sell and all units meet the full design standards necessary for the private market. In addition, some shared amenities are being provided, including a concierge and laundry room at ground level and at first floor an amenity room and a meeting room. These amenities and the shared entrance they surround will add activity and an active street frontage. Combined with a series of small retail units face onto Market Street, Block 5 will contribute to the activity and street life along the busy Main Street which is an important pedestrian and cycle priority link from Fr. Collins Park to Clongriffin Station Square.

The proposed residential mix is 52no. 1 bed units, 83no. 2 bed units and 3no. 3 bed units. This generates a mix of 38% 1 beds, 60% 2 beds and 2% 3 beds.

The proposed commercial offer includes four separate small retail units on Market Street. These will provide important street frontage and activity onto this street.

Residential External amenity space is provided at podium level in a shared courtyard 879m<sup>2</sup> in size. An additional roof garden at fourth floor level is 682m<sup>2</sup> in size.



Block 5 west;  
4 floors of residential overlooking the park with highly landscaped roof terrace above.

Block 5 South;  
Retail uses at ground level with two floors of residential above.

Block 5 east;  
5-6 floors of residential units with set pack penthouse levels



3D view of Block 5

**Site Summary:** Build to sell

Total Residential Units	138 nr. (397 bed spaces)
Total Commercial Area	393sqm (gross)
Site Area	0.70 ha (1.72 acres)
Plot Ratio	2.13
Site Coverage	100%
Net Density	197 units/ha (80 units/acre)
Building Height	3 to 6 storeys + penthouse
External Amenity Space	1,561 sqm (including roof terrace)
Parking	54 spaces off-street, under podium 42 spaces on-street 96 Total
Cycle Parking	224 spaces off-street

**Schedule of Accommodation**

1 Bed Units	52	(55.1sqm each)
2 Bed Units	83	(86.3sqm each)
3 Bed Units	3	(117.4sqm each)
<b>Total</b>	<b>138</b>	
<b>Retail</b>	<b>393 sqm (gross)</b>	
<b>Total</b>	<b>393 sqm (gross)</b>	



## 5.3 Block 14 - Overview



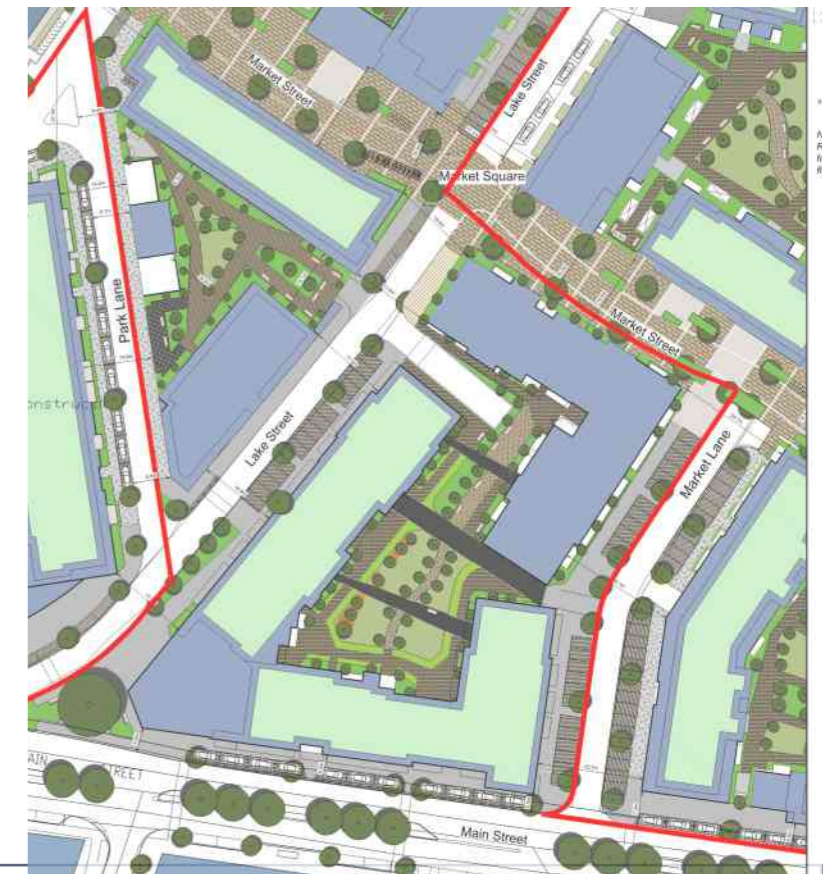
Block 14 is a 0.97 hectare site located at the heart of Clongriffin enclosed by Main street, Market Street, Market Lane and Lake Street.

Block 14 is made up of two blocks which enclose a landscaped courtyard. Block A contains 97 apartments over six floors, over basement. Block B of eight floors over basement, contains a mix of café / restaurant space, commercial / retail and residential uses with 191 apartments, 1127m<sup>2</sup> of commercial / retail space and 806m<sup>2</sup> of café / restaurant space. In addition, some shared amenities are being provided including a gym, cinema room, multimedia room, residents lounge / function room, resident's business room, meeting room, laundry, bulk storage and concierge. These facilities are located on the Ground Floor and are easily accessible from Market Lane or from the communal courtyard.

The commercial / retail and café / restaurant space of Block B face onto Main Street and Lake Street creating an active street frontage.

Block B addresses the corner of Main Street and Lake Street. Rising to eight storeys the façade is made up of facets of stone cladding with varying window sizes. The Ground Floor contains a double height café / restaurant with glazed street frontage.

The proposed residential mix, over both blocks A and B comprises of 49 studio apartments, 106 one bed apartments, 121 two bed units and 12 three bed apartments, totalling 288 units.



### Site Summary: Social Housing and Build to Rent

Total Residential Units	288 nr. (861 bedspaces) (97 Part V, 191 BTR)
Total Commercial Area	1,933sqm (gross)
Site Area	0.97 ha (2.4 acres)
Plot Ratio	2.7
Site Coverage	47%
Net Density	296 units/ha (121 units/acre)
Building Height	6 to 8 storeys
External Amenity Space	1,977 sqm
Internal Amenity Space	745 sqm
Parking	162 spaces off-street, under podium 49 spaces on-street 211 Total
Cycle Parking	651 spaces off-street

### Schedule of Accommodation

Studios	49
1 Bed Units	106
2 Bed Units	121
3 Bed Units	12
<b>Total</b>	<b>288</b>

Commercial/ Retail	1,127 sqm (gross)
Café/ Restaurant	806 sqm (gross)
<b>Total</b>	<b>1,933sqm (gross)</b>



## 6.0 SOCIAL INFRASTRUCTURE

### 6.1 Public Open Space & Amenity

An important aspect of the master plan proposal is the links and connections between open spaces. Each space provides value in its own right and together they connect to create a permeable, human scale, vibrant neighbourhood.

The original planning application (Reg Ref 0132/02) was granted for 3,576 units. The majority of this open space associated with this application has already been provided through the River Mayne Corridor, Station Square, financial contributions to Father Collins Park, Bellpark Square and Beau Park. The balance will be provided in a concurrent application; 2,634m<sup>2</sup> open space being provided through 1,434m<sup>2</sup> in Grant Park and 1,200m<sup>2</sup> in Railway Park.

The LAP makes provision in Section 7.7 for open space provision is less than the development plan range of 12-15sqm per bedspace. This is in order to achieve the sustainable net residential density targets in the LAP. Precedent in Clongriffin has been 10sqm per bedspace and the required quantum is identified this way. There is an overall over-provision of open space and this has been biased towards public use in recognition of the reality that Fr Collins Park will be the first open space destination of choice for the majority of residents. The proposals are also in close proximity to the River Mayne linear park and open space around the attenuation basins.

Public open space in the parent permission was at a minimum rate of 10% of the site area and has been provided in Beupark, Station Square, the 'Panhandle Park' (now Beltree Park), the Mayne River linear park and a number of other pocket parks throughout the site. Fr. Collins Park is a high quality amenity for Clongriffin, especially the new homes beside it.

In designing urban environments, the quality of open space is paramount. Open space and greenery can provide visual relief and variety to members of the public and residents within the town. The subject application proposes a high quality, pedestrian priority, public realm. This is the approach in all aspects of the public realm, but especially so in Market Street.

Consideration has been given to the entire experience of living in and visiting Clongriffin. Where possible glimpsed views into some podium courtyards are provided from street level, long and short vistas within the town have been set up to provide structure and aid way-finding, materials have been carefully selected to enhance the pedestrian experience and street trees provide visual relief and diversity in the streetscape. The overall impression within the town centre will be of a very green setting.

For full details please refer to the Landscape Architects report.

Photo of the Attenuation Pond, March 2019, right.

Context plan showing open spaces within the subject application and in the wider area, below.







Diagram showing the network of open spaces in the proposals linking into the wider context



## 6.2 Community Facilities

Clongriffin Masterplan makes provision for an array of community uses and facilities throughout the Town Centre. These include health centres, a community centre, a Mens Shed, sports clubs, an Islamic Centre and several flexible spaces available for hire for classes and community uses. Many of these services have already been delivered in earlier phases. Block 4, within the subject application, will provide a hub of community services.

Section 12.5.3 of the Dublin City Development Plan states that "A range of community facilities and infrastructure will be essential to support the emergence of sustainable neighbourhoods and communities throughout the city, especially in newly emerging or developing areas". Gerard Gannon Properties currently provide a temporary community centre as well as a Men's Shed for Clongriffin residents and the new Block 4 community centre will replace these temporary spaces with purpose built facilities.

The permanent community centre will be conveniently located at the prime corner of the Block 4 site as one approaches from Main Street, establishing itself as a new hub for the existing local community. The permanent Mens Shed will be located adjacent, with a room and small open space available. The symbiotic association between the community centre, cafe, childcare facility, Men's Shed and BTR accommodation will serve to support the mutual success of all these uses.

The site's proximity to the pedestrian friendly Market Street, linking Station Square with Fr. Collin's Park, and it's location along the midpoint of this route also adds to the suitability of this site for community centre use. The proximity to public transport routes by rail and bus make the community centre easily accessible by the wider community of Clongriffin and Belmayne also.

The design of the community centre space is kept as flexible as possible to allow for multifunctional future use and possible subdivision if required. Indicative seating layouts allow up to 132 seats in the space. A stage area is also indicated but this could be a temporary or removable structure depending on the community needs. A tea station is provided for events as well as adequate toilet facilities.

The community hub in Block 4 will benefit all residents of Clongriffin, both future and existing.

The community audit prepared by the planning consultants Downey Planning and submitted as part of this application will analyse the needs and proposed provision for community overall.









# 6.3 Education & Childcare

Clongriffin has developed into an ideal location to raise and educate a family, with abundant amenities right on the doorstep. Three creches are already in operation in the town and proving successful. A further three creches, located in Blocks 4, 6 and 27, are proposed as part of this and concurrent applications. This will complete the required creche provision for Clongriffin.

A key element of the Masterplan is the provision of adequate childcare facilities to serve the overall development. It is important to note that there are 13 childcare facilities within 2 km of the subject application. Permission has also been granted for a childcare facility accommodating 50+ children on Marrisfield Avenue that is currently vacant.

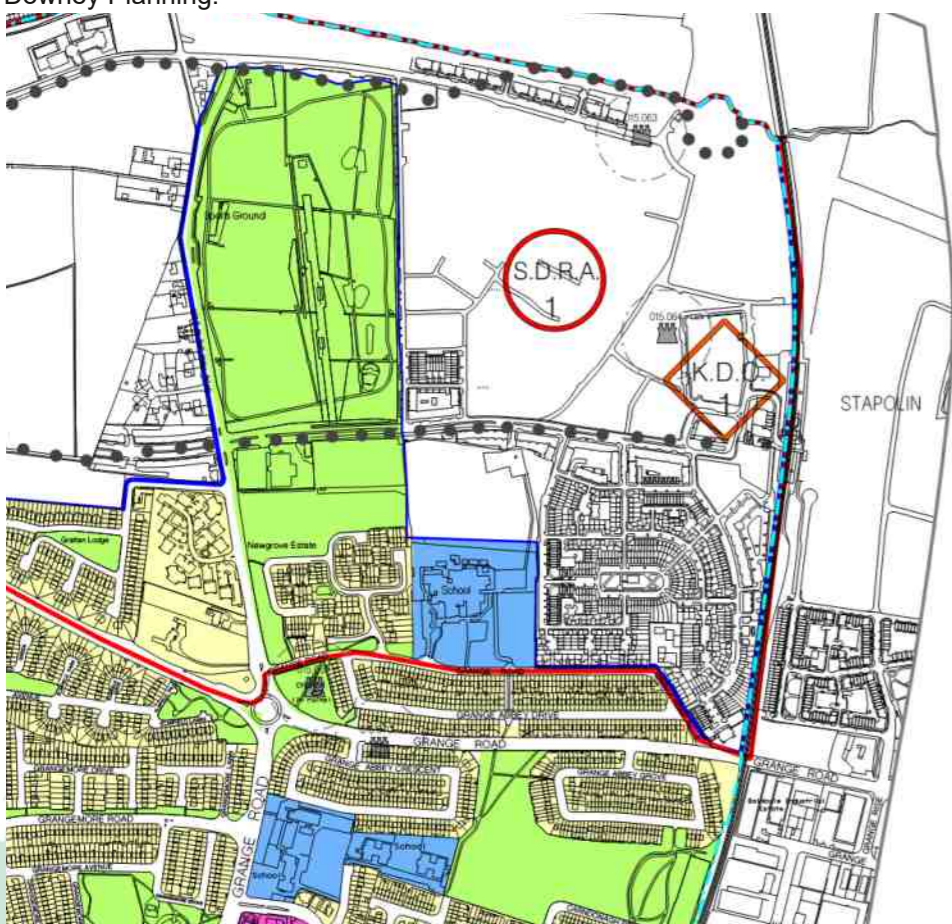
Block 8, within the subject application is indicated as a suitable site for an education use on one map within the Clongriffin-Belmayne LAP, however, the location of this education use is not current specified in the DCC SDRA 1 Map. A more suitable site has been identified adjacent to the playing fields, central to the wider community, and is indicated on the drawings to the right.

Land to the south of Main Street is zoned as Educational use in the DCC Development Plan 2016-2022. The proposed site for the school is to the north of the Grange Community College. This location is ideal for a school as it would provide a civic front to Main Street, allow community use of the school hall for local events, be appropriately sited close to the playing fields and leisure centre and also be located centrally to the wider community adjacent to excellent transport links. More detail on the Education zoning is provided in the Block 8 Architect's Report and Planning Report accompanying this application.

The provision of childcare spaces has been addressed separately in a report by Downey Planning.

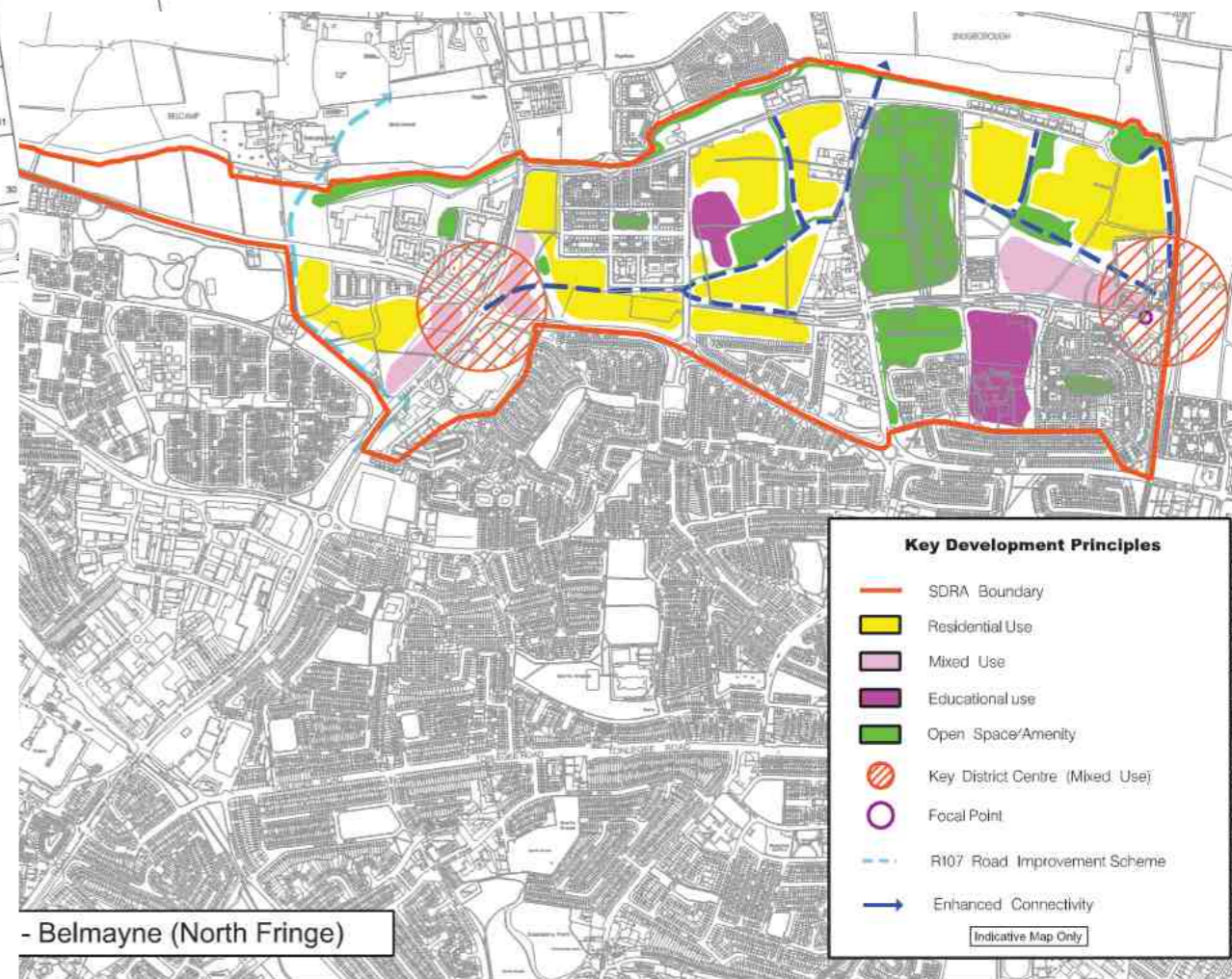


Layout of creche in Block 4, showing spaces laid out over two floors with rooms arranged around an external play space at ground floor



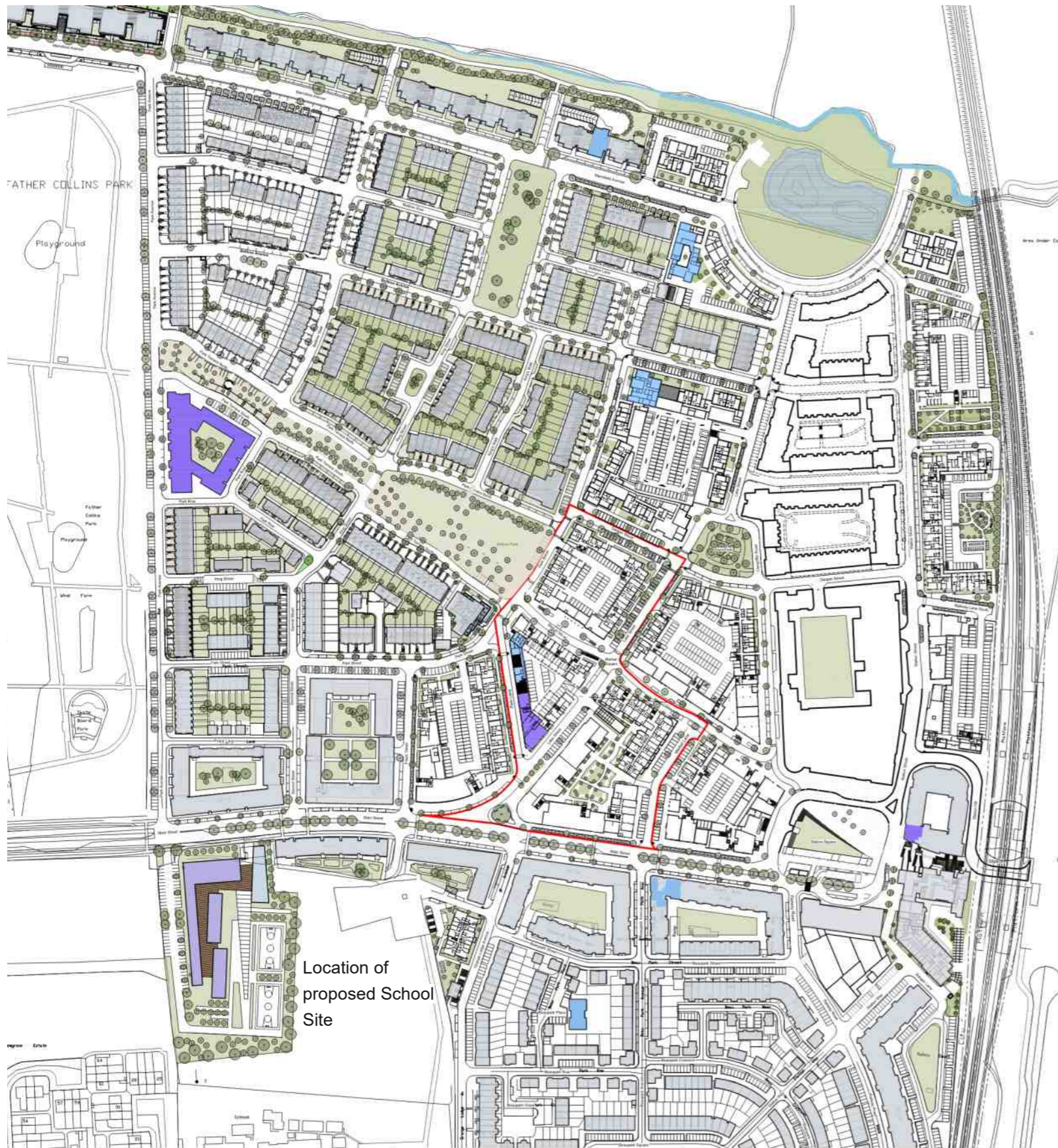
Extract from DCC Map C, left, showing the locations of SDRA 1 and KDC 1, with education use indicated to the south of the proposed school site, on Grange Abbey Road.

Extract from SDRA 1 Clongriffin - Belmayne (North Fringe) map, right, from Dublin City Development Plan 2016-2022



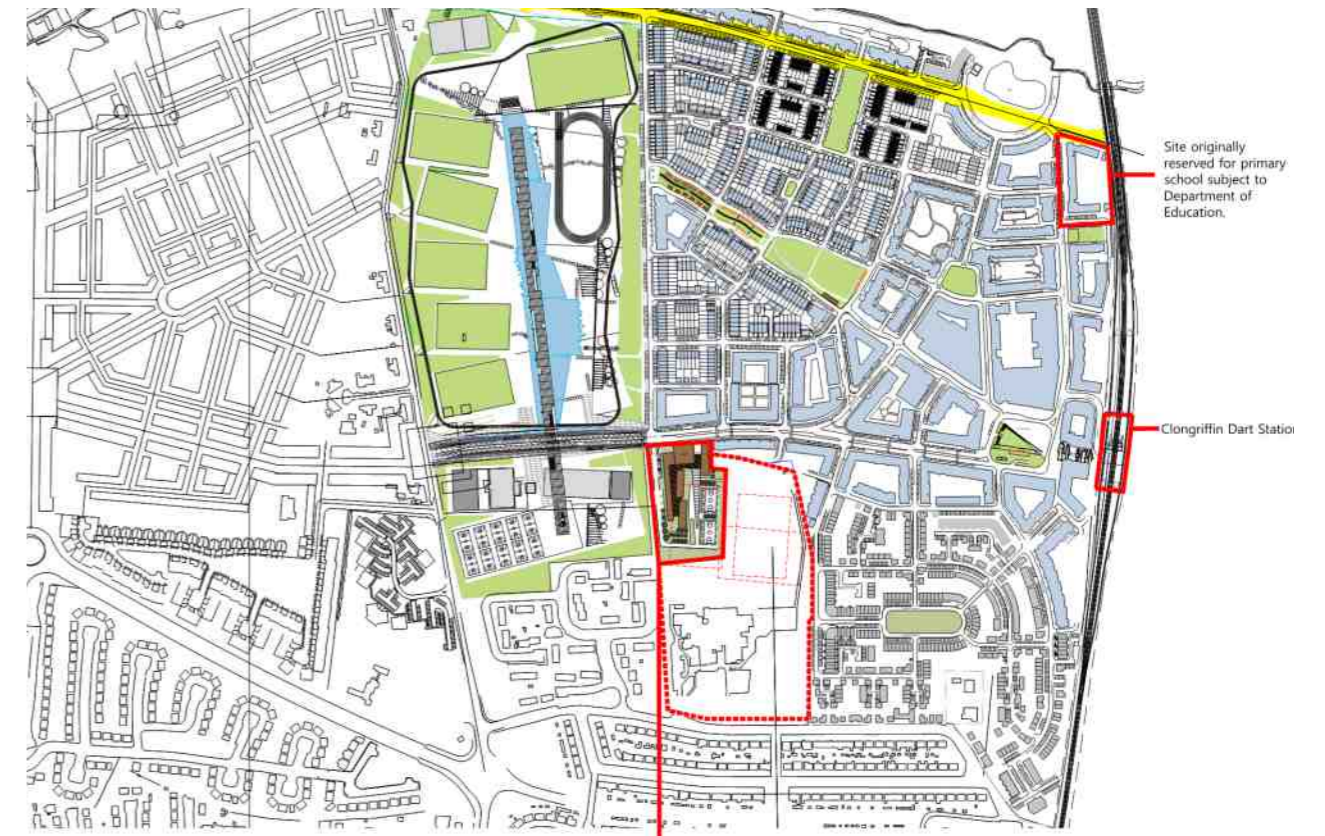
- Belmayne (North Fringe)



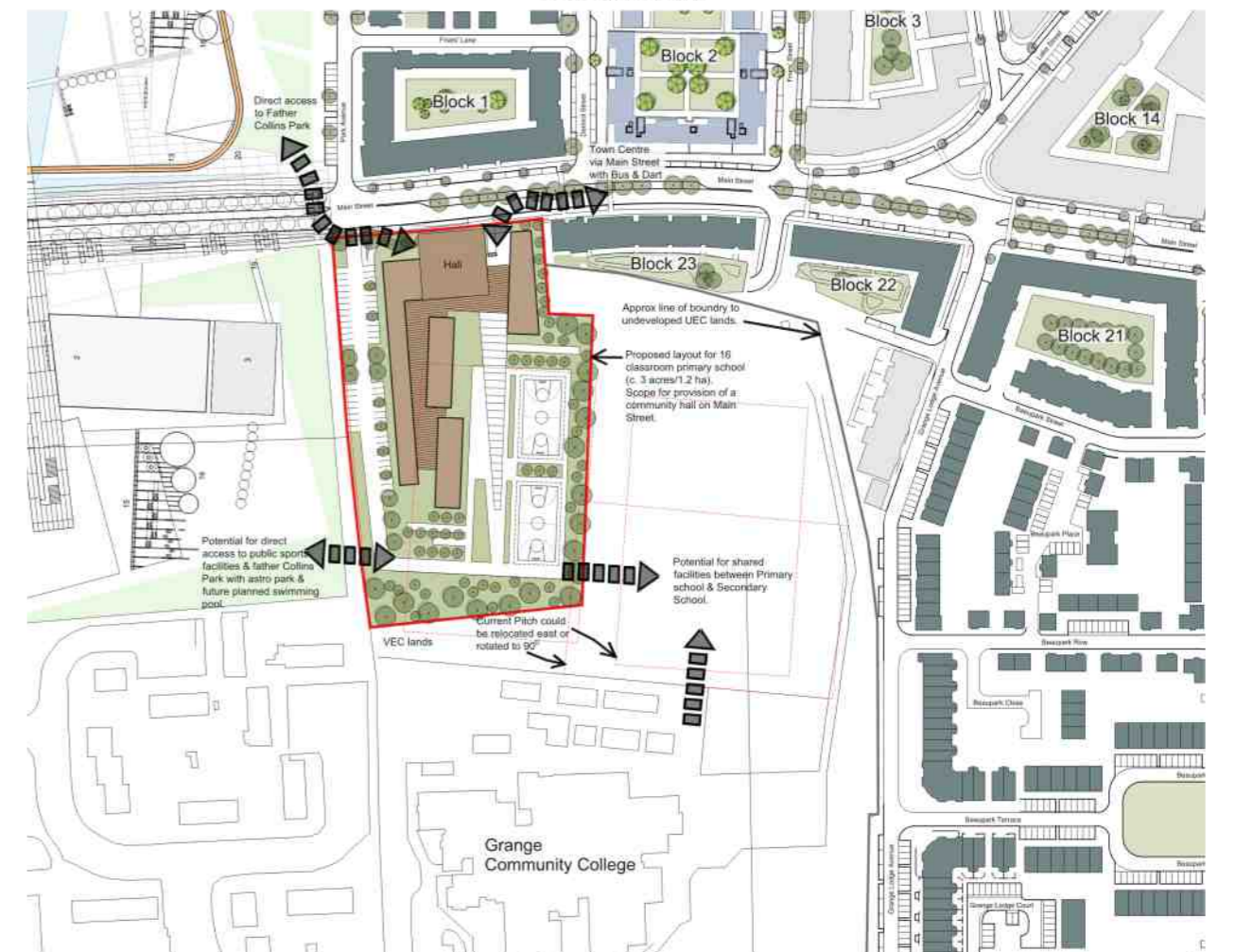


Location of creches in the masterplan.

An outline design for a school is identified on the zoned site and shown to the right. This location is ideally situated central to the wider community. A school in this location would provide a good civil frontage onto Main Street at a key node and be ideally located next to the existing playing fields and leisure centre



Potential Site of new primary school next to Father Collins Park, Bus Stops, Shared facilities with adjacent Grange Community College.



Sketch proposal for the school site in Clongriffin



## 6.4 Commercial & Retail

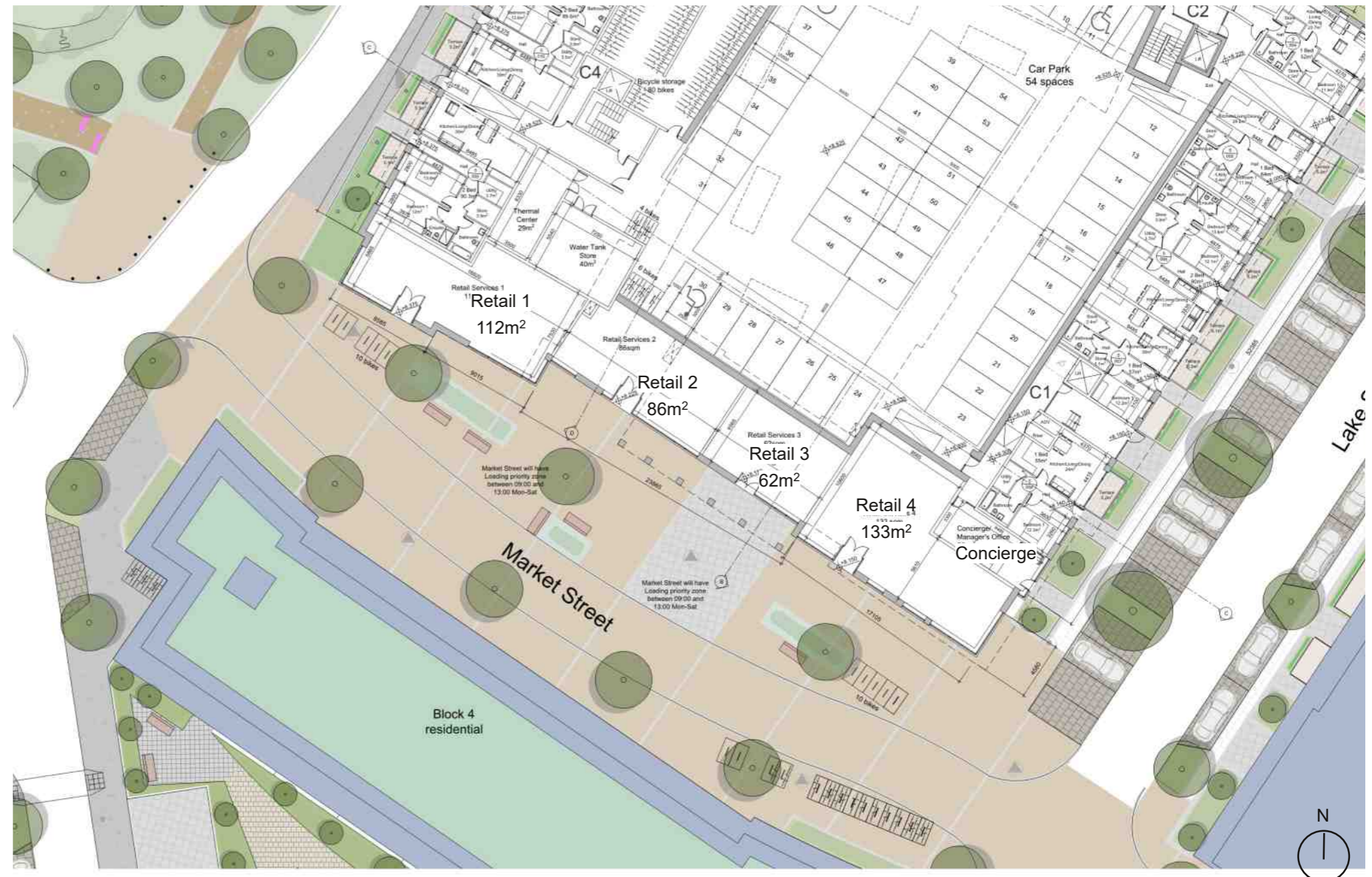
A mixed-use, vibrant commercial centre is central to the success and future of Clongriffin Town. 13,950m<sup>2</sup> of commercial space has already been delivered or is currently under construction at Clongriffin.

Blocks 12 and 16 were complete in 2007-08 and many of the commercial units are currently operational. This includes restaurants, financial services, a gym, an Islamic Centre, several creches, healthcare providers and shops. To date these commercial uses have clustered around Main Street and Station Square. The combination of high quality public realm and proximity to frequent public transport has created a thriving local centre with plenty of amenities for residents.

The final phases of Clongriffin, outlined in this and concurrent applications, will bring the critical mass of population required to support the next wave of commercial uses. The concurrent applications propose the final commercial blocks surrounding Station Square, in Blocks 13 and 15. There are also office and retail uses proposed in Block 3, on the junction of Main Street and Lake Street. Blocks 17 and 28 also propose some convenience retail uses and a gym adjacent to Bridge Street and Station Square. Blocks 4, 5 and 14, part of the subject application, are also an important part of this story.

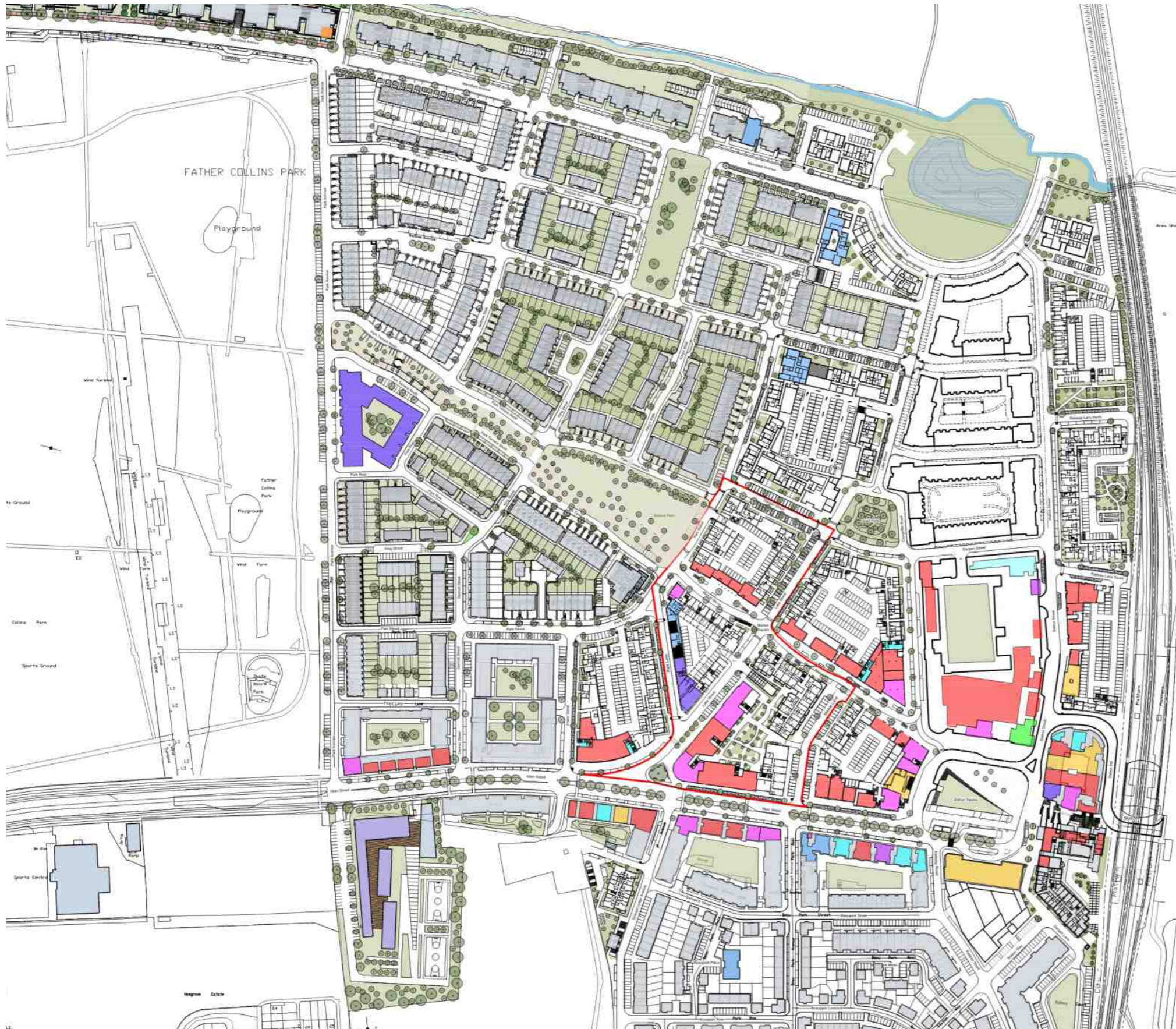
A total of 22,727.5m<sup>2</sup> of commercial space is proposed across the three concurrent applications. This includes retail, office, restaurants and a cinema. Of this total quantum, 3,125m<sup>2</sup> is within the subject application. This application focuses on residential development. Whilst commercial uses are not the focus of the application, the future success of Clongriffin as a local town centre and mixed-use hub is dependent on the density of development proposed, and the subject application plays an important role in the making of this viable new town centre.

The three blocks play an important role in the framing and success of Market Street. This street will primarily be used by pedestrians and active frontages along its length will be important to the vibrancy of this street. Different sizes of units have been proposed, to allow a diversity of commercial occupiers, increasing amenity for residents who will be using the street.



Retail units at ground floor, Block 5, with frontage onto Market Street





Screengrab from ClongriffinTown.ie, a website that keeps local residents up to date with what's going on in Clongriffin.

- Shop
- Restaurant/cafe
- Professional Services
- Community Services
- Retail
- Financial Services
- Pub
- Leisure
- Off-licence
- Creche

Commercial and community uses  
Existing and Proposed



## 6.5 Social / Part V

Gerard Gannon Properties have an ongoing commitment to provide the agreed percentage of social and affordable housing in Clongriffin. Care has been taken to ensure that social units are delivered in an integrated, tenure blind manner.

It is also an aspiration to have the units distributed as widely as possible across the Town. In practice this has proven difficult in some of the larger blocks to subdivide the building in terms of management. The majority of blocks exceed 100 units. Discussion with social housing providers indicates that there is a preference for distinct blocks with their own core and amenity areas, as this facilitates streamlined management.

Block 2 (Reg Ref. 3776/15), is the first social housing apartment block to be delivered in Clongriffin, it will be complete in early 2019 and deliver 109 units. It is intended that the Iveagh Trust will take over management of the building on completion. They already successfully manage a block of 36 apartments on Marrsfield Avenue and a block of 36 duplex and apartments in Beaupark. The Iveagh Trust look for a minimum number of units on a single site to make the scheme manageable and efficient, usually between 50-75 units, which they will maintain an office and a live in caretaker.

Two more blocks of social housing are proposed in Clongriffin, to complete the requirement for social homes in the town. One of these blocks fall into the subject application and the balance are in a concurrent application.

This application includes 97 social homes in Block 14 north. This represents 19% of the units in this application.

Concurrent applications include one full block of social homes; Block 11, and two units within Block 29. These blocks contain 96 and 2 units respectively, with 98 in total outside this application. This will complete the social housing provision for Clongriffin, with a total of 195 units provided across the three concurrent applications, representing 10% of the units proposed.

All social units have been designed in line with the guidelines in Sustainable Urban Housing: Design Standards for New Apartments (March 2018). All apartments include open plan kitchen/ living/ dining areas and private balconies and roof terraces. Living spaces are generously designed with aspect maximised where possible and large windows providing lots of light. Bicycle parking and communal open spaces have also been designed to comply with the guidelines. In addition, all residents will benefit from the same social and transport infrastructure as surrounding BTR residents including the proximity of Clongriffin Station, Dublin Bus services, a Community Centre, creches, shops and other commercial units.

Clongriffin SHD 2 - Social Housing Units					
	1 Bed	2 Bed	3 Bed	Studio	TOTAL
Block 14A	42	45	6	4	97
%	43%	47%	6%	4%	100%



View of Block 14





**Allocation of Social Housing Units in the three concurrent applications**  
**Part V - Total 195 Units**

**SHD 1 - 98 Units**

Block 11: 96 Part V Apartments  
 44x 1 Bedroom Apartments, 12x 2 Bedroom Apartments, 40 x 3 Bedroom Apartments

Block 29: 2 Part V Apartments  
 1 x 1 Bedroom Apartment, 1 x 2 Bedroom Apartment

**SHD 2 - 97 Units**

Block 14: 97 Part V Apartments  
 4x Studio Apartments, 42 x 1 Bedroom Apartments, 45 x 2 Bedroom Apartments, 6 x 3 Bedroom Apartments



# 7.0 DEVELOPMENT STANDARDS

## 7.1 Residential Density & Dwelling Mix

Density varies across the town centre, increasing with proximity to the station. Overall an average density in excess of 88dph is anticipated, once this and concurrent applications are built out. This density is above average for similar locations around Dublin and represents a highly sustainable density, particularly in the context of the high number of family houses prevalent.

The proposed density helps to create a sustainable community and supports commercial and community uses including creches, pubs, shops, professional services, restaurants/ cafés, a yoga studio and community centre. A large local population combined with walkable and bikeable streets combine to allow small businesses and local shops to thrive.

Clongriffin already has a population of 5,000 residents with approximately half the residential units built to date. Already the population has created sufficient momentum to support significant commercial and community activities.

This density has been possible as a result of the exceptional public transport infrastructure including Clongriffin Train Station, the Malahide Road QBC and a network of cycle tracks linking Clongriffin the 9.4km to the City Centre.

A variety of building types have been provided and allowed for Clongriffin. Typologies vary from two and three storey terraced homes on neighbourhood streets, to higher density apartments and duplexes in shared and mixed-use blocks. As earlier phases have focussed on building medium density, single family homes, the subject application proposes a higher density of perimeter blocks of apartments. This allows an average density sufficiently high to sustain a mixed-use town centre.

This critical mass will be the key to the success of Clongriffin as a sustainable, mixed-use, town centre.

Density has many benefits for the local populations including;

- Increased value
- Convenient shops and services
- Safer Streets
- Design for Living
- Energy Conservation
- Mixed Communities
- 'Somewhere not Anywhere'

The density of Clongriffin SHD2 is 200.5dph

No. of blocks:	3
Total no. of residential units proposed:	500
Density proposed (dwellings per hectare):	200dph
Plot ratio of SHD 1:	2.08
Total site coverage:	71.34%
No. of social housing units:	97 (19.4%)
No. of build to rent units:	265 (53%)
No. of build to sell units:	138 (27.6%)
Total floor area proposed:	51,840m <sup>2</sup>
Area of land subject to this application:	2.49Ha

### Breakdown per Block

Certain assumptions have been made when calculating the density of the individual blocks in Clongriffin. These are laid out below. All assumptions are based on the DOELG guidance document 'Residential Density, Guidelines for Planning Authorities' (September 1999).

1. Site areas are taken to the centre of surrounding roads.
2. Plot ratio is calculated as the total floor area (residential and commercial) divided by the site area.
3. Site coverage is calculated as the built footprint (including podium parking) as a percentage of the site to the back of pavement.

Apartment Block	1 Bed Units	2 Bed Units	3 Bed Units	Studios	Total
Block 4	9	55	10	0	74
Block 5	52	83	3	0	138
Block 14	106	121	12	49	288
<b>TOTAL</b>	<b>167</b>	<b>259</b>	<b>25</b>	<b>49</b>	<b>500</b>

"Our common goal is to create better neighbourhoods, and higher densities should be seen as the means rather than the end."

-Better Neighbourhoods: Making higher densities work, CABE

### Dwelling Mix

	No. of dwellings	%
1 Bedroom Apartment	167	33.4%
2 Bedroom Apartment	259	51.8%
3 Bedroom Apartment	25	5%
Studio	49	9.8%
<b>Total</b>	<b>500</b>	<b>100%</b>





Walking distances from Clongriffin Station



# 7.2 Parking Strategy

Car parking will be provided at a reduced rate across the masterplan, as discussed and agreed with DCC. Dublin Bus are running services through the lands, linking to the Malahide Road QBC, and train services have increased at Clongriffin Station. Principles from DMURS have been incorporated, such as tree planting, on-street car parking and curved roads, to aid traffic calming, reduce speeds and prioritise pedestrians and cyclists whilst accommodating cars.

Different approaches to car parking have been taken across the masterplan, depending on the nature of the street. A mixture of podium, basement and on-street car parking have been employed in order to develop a hierarchy of streets, from neighbourhood to town centre.

The overall development has been divided in 3 zones, which relate to the distance to Clongriffin Train Station; Red Zone: within 200m from the train station (5m walk approx), Green Zone: within 375m from the train station (7m walk approx), Blue Zone: within 500m from the train station (10m walk approx). Based on the different zones, a different rate has been used to estimate the residential car parking provision from a rate of 0.65 (Red Zone) up to 0.75 for Residential units within Blue Zone. The commercial car parking provision is based on the maximum car parking standards for various land uses. The total provided represents a reduction of about 25% from the maximum. A 483 space park and ride facility has also been constructed and is in operation beside the railway station. Additional car share vehicles are proposed as part of this and concurrent applications, the details of which can be found in the Engineer's report.

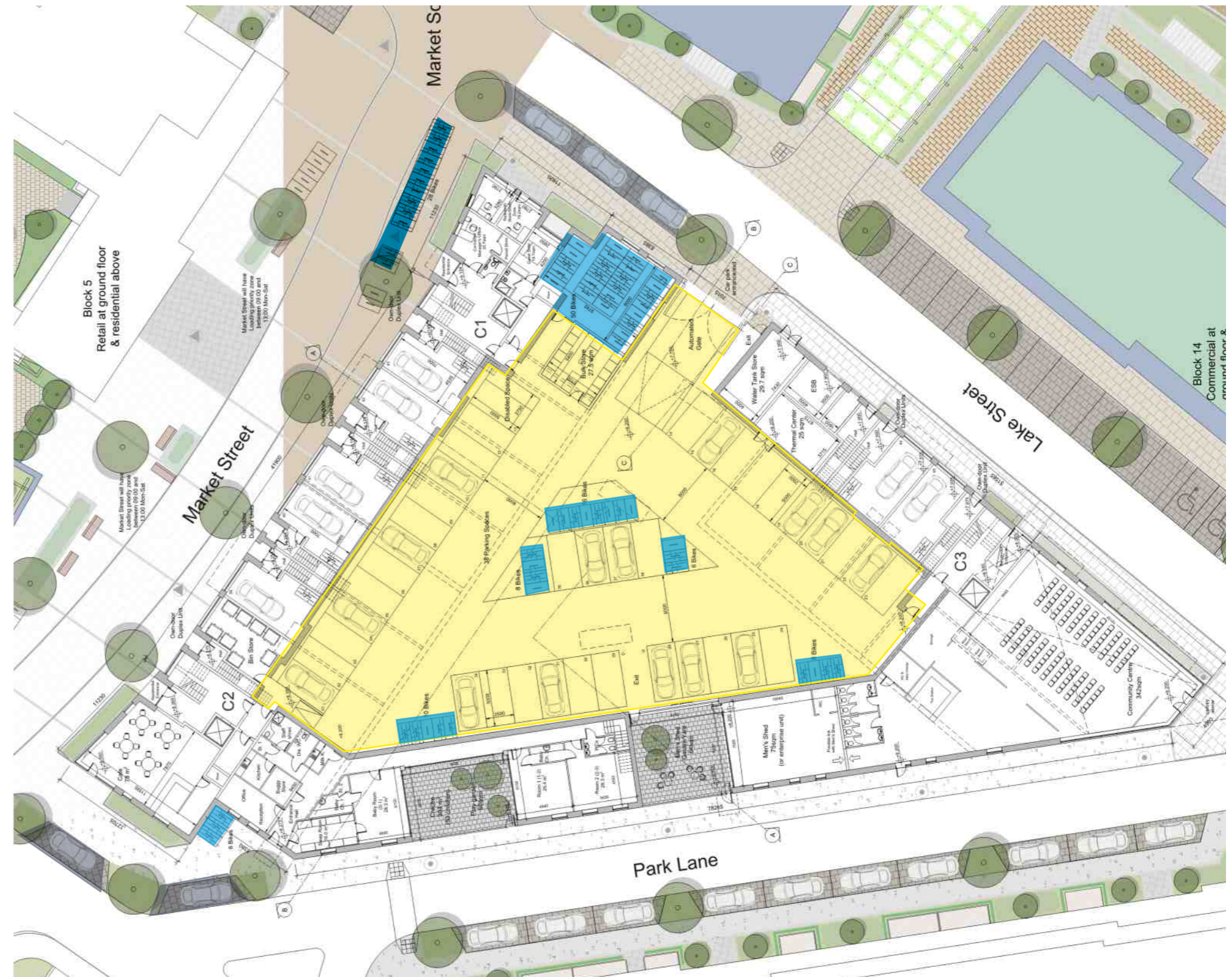
The intention is for the street network to be taken in charge by the local authority and all roads and footpaths have been designed accordingly. Please see the engineers report for further detail.

A total of 357 car parking spaces are provided in the subject application, divided between the three blocks. This equates to a ratio of 0.7 spaces per unit. This ratio has been agreed on after discussion with the local authority, and taking into account the proximity to Clongriffin Dart Station, local bus services and bicycle parking provision.

The large commuter car park in Block 12 has capacity to accommodate visitors and overflow resident use. Car share vehicles already located in this car park have proven very popular with the existing population and the scheme was recently extended.

Further details on car parking provision is available in the engineers report.

- Surface parking
- Street Parking
- Bicycle Parking



Block 4 parking plan





## 7.3 Public Transport & Cycling

Public transport in Clongriffin is already extremely good, with a frequent DART service, running every half hour during peak hours. The No. 15 bus runs 8 services per hour during the peak and every 8-12 minutes thereafter.

Public transport services are expected to improve again once further phases of development are complete. Furthermore, Clongriffin is the start point for the first route proposed in the new Bus Connects Strategy from the NTA. This would see dedicated bus and cycle corridors constructed, all the way from Clongriffin to the City Centre and could cut journey times 65 minutes to 30-35 minutes.

A total of 1,007 resident bicycle parking spaces are provided in the subject application. This allows 1.86 bicycle spaces per apartment. Whilst the guidelines set out in Sustainable Urban Housing: Guidelines for Planning Authorities (March 2018) suggest a higher provision this number is considered sufficient when taken in the context of local walking links, bus stops and Clongriffin Station.

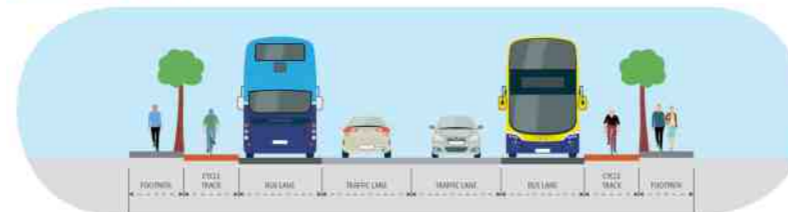
Resident bicycle parking has mostly been located within the podium and basement car parks, where it will be secure and covered. Access to the car park will be restricted to residents only and the location of bicycles adjacent to cars should provide for passive surveillance and security for parked bicycles.

The cycle network for Clongriffin to surrounding areas is already excellent, with segregated routes available along the Malahide Road, locally within the Town Centre and along much of the coast. At 9.4km to the city centre Clongriffin is within reasonable commuting distance for many people and high uptake of cycling as a primary mode of transport is anticipated.



Bus Connects Route 01; Clongriffin to City Centre

Optimum Road Layout





## 7.4 Private & Communal Open Space



Block 4 landscaped podium view

Private balcony and terrace sizes of apartments are indicated on the Floor Plans drawings, and generally exceed the minimum requirements. Apartments have all been provided with private balconies and/or terraces in accordance with DoEHLG Sustainable Urban Housing: Design Standards for New Apartments (March 2018).

Balconies and terraces shall be screened with glass and aluminium screening, as per the proposed building design. At first floor balconies will be opaque glass to protect privacy. Ground floor terraces shall be protected with low hedges and soft landscaping.

Shared private amenity space is provided within the curtilage of each block, in accordance with DoEHLG Sustainable Urban Housing: Design Standards for New Apartments (March 2018). Communal open spaces are provided in large, communal, landscaped, podium courtyards in the centre each block.

The total requirement for communal open space is 3,069m<sup>2</sup> and the proposed development provides 4,560m<sup>2</sup>, which is almost 1.5 times the minimum requirement. This is spread across all blocks, with each meeting its own requirements locally. More detail is provided in the individual design reports.

Living roofs will also be employed at roof level of many blocks. A roof terrace is provided in Blocks 5. All apartments will be bright and airy with plentiful light from full height glazing.

In addition to external amenity space all BTR Blocks are provided with internal amenity spaces and BTR Support.

### Communal Open Space Requirement

	1 Bed	2 Bed	3 Bed	Studio	TOTAL
<b>Number</b>	167	259	25	49	500
<b>Area Requirement</b>	5	7	9	4	
<b>TOTAL</b>	835	1,813	225	196	3,069

### Communal Open Space Provision

Space	Area (m <sup>2</sup> )
Block 4 Landscaped Podium Courtyard	1,022
Block 5 Landscaped Podium Courtyard & Roof Terrace	1,561
Block 14 Landscaped Surface Level Garden	1,977
<b>TOTAL</b>	<b>4,560</b>





Landscape Architect's drawing showing living roofs and landscaped podium courtyards across the application



## 7.5 Aspect & Separation Distances

The proposals have been designed to maximise opportunities for dual and triple aspect units, whilst also balancing this desire with maintaining an appropriate density to sustain this emerging urban centre and creating strong street frontages through perimeter blocks.

Where possible, views over amenity space and pedestrian Market Street have been maximised. Blocks 4 and 5 all overlook the high quality pedestrian environment of Market Street.

Dual aspect units make up the majority of units at 54%. As outlined earlier in this report, a minimum of 50% is acceptable in this urban location. In fact, 33% dual aspect ratio is acceptable in accessible locations as per details in Sustainable Urban housing: Guidelines for Planning Authorities (March 2018). No north facing single aspect units are proposed.

All apartments have been designed with full height glazing, private outdoor spaces and open plan living spaces, designed to maximise light penetration and liveability. Floor to ceiling height of at least 2.6m has been achieved in most blocks, further increasing daylight penetration into living areas.

### Orientation of Single Aspect Units

Block	No. of Dual Aspect Units	% of Dual Aspect Units	No. of Single Aspect Units	No. of North Aspect Single Aspect Units	View/ Amenity of North Aspect Single Aspect Units
Block 4	47	64%	27	0	n/a
Block 5	75	54%	63	0	n/a
Block 14	145	51%	143	0	n/a
<b>TOTAL</b>	267	54%	233	0	



## 7.6 Universal Design

Each of the three blocks and all the public realm has been laid out to enable easy access by all and fully comply with Part M of the Building Regulations. Building for Everyone: A Universal Design Approach has been used as a guideline for both external and internal environment design.

All main entrances to buildings will be fully accessible including, their approach.

The design of the public realm is based on the DMURS standards. Priority is given to pedestrian use of the space. All surfaces within the application boundary will be level (up to 1:50) or gently sloped (up to 1:20), to ensure the space is usable by those of various abilities. All access routes will be well lit and surfaced with firm and reasonably smooth materials having the appropriate slip resistance. Drainage gratings will be flush with the surrounding surface. The footpaths will be sufficiently wide to accommodate all users, from students with bicycles, to parents with buggies, to the elderly.

Adequate dishing at kerbs and safe pedestrian crossing areas will be provided at all junctions around the site. Tactile paving surfaces will be used where necessary to warn of the absence of a kerb.

All guarding and handrails will full comply with Part M requirements, where relevant on access ramps. Several accessible parking spaces are provided across the proposals, both on-street and within private car parks. The design of on-street parking stalls complies with the guidelines in Building for Everyone: A Universal Design Approach, Section 1.

The site landscape design includes public benches to provide resting spaces for the public. Routes within the buildings are kept simple and legible for patrons of all ages and abilities. Lighting and signage will be designed to enhance the environment for all users.

Entrances to all buildings will be accessible. They will be easily identifiable, with level landings of 1800 mm x 1800 mm immediately in front. Suitable surface materials will be used so as not to impede wheelchair users nor create a tripping hazard. General circulation corridors will be minimum 1500m wide with wheelchair passing places 1800mm x 1800mm at intervals of maximum 20 metres but generally much more frequent.

Passenger lifts are provided to all accommodation units as well as to all shared BTR facilities, concierges, communal amenity spaces, bin stores and roof terraces as relevant. All exit stairs from upper levels are designed to be accessible for ambulant disabled.





## 7.7 NZEB (Nearly Zero Energy Buildings)

Clongriffin will be quite unique within the greater Dublin area and within Ireland with the planned de-centralised heating network which is proposed for the new development. This will be known as the Clongriffin District Heating Network (CDHN). District heating offers not only advantages in terms of higher efficiencies and reduced consumption but also on capital cost savings and reduced operating and maintenance costs. District heating networks are very common in Europe and some North American cities and the aim is to help Dublin become a leader in sustainable energy.

The Clongriffin District Heating network allows for efficient use of thermal energy using mainly combined heat and power giving the project a 23% energy saving and a 38% carbon dioxide saving. The proposed CDHN also provides the main input for compliance to the new Nearly Zero Energy Building requirements for both the residential (Part L 2018) and commercial (Part L 2017) elements. The CDHN will also fulfill renewable energy requirements under Part L 2017/18 thus saving in capital cost, roof space requirements and maintenance of photovoltaic and solar thermal installations. This has an added benefit of allowing extensive areas of roofs to be dedicated to extensive sedum roofs and roof gardens to aid SuDS and drainage measures.

### Introduction.

This project note is a high level summary of the proposed global approach to sustainable energy usage in the dwellings and commercial units.

### Methodology.

The approach to the global analysis across all of the blocks was to cover the total combined residential and commercial area using a number typical sized unit making up and equalling the total combined residential and commercial areas.

### Statutory requirements.

Given the current time frame compliance would need to be in line with :

Residential element : Part L 2018 and applies to all works commencing after 31-03-2019. (currently Part L 2011)

Commercial element : Part L 2017 applies to all works commencing after 31-12-2018.

Part L 2018 will replace Part L 2011 once the public consultation process is completed. (NZEB)

Part L 2108 is also referred too as the "Nearly Zero Energy Building" (NZEB)

### Unit configuration.

The Clongriffin district heating network (CDHN) energy calculations covers the following units and areas ;

Blocks 3,4,5,6,8,11,12, 13, 14, 15, 17, 19, 25, 26, 27 and 28 :

Residential units :	1,950 no	Residential area :
	125,860 m <sup>2</sup>	
Commercial units :	+/- 80 no	Commercial area :
		22,750 m <sup>2</sup>

### BER target.

The typical target BER label for a Part L 2018 (residential) compliant unit will be : high A2 or low A3

pending the configuration of the unit.

The typical target BER label for a Part L 2017 (commercial) compliant unit will be : high A3 or low B1

pending the actual use of the unit.

### Part L Compliance parameters.

The following measures have been applied to achieve compliance to TGD Part L 2018 (residential )and Part L 2017 (commercial)

- a. Ground floors :  $U \leq 0.110 \text{ W/m}^2\text{K}$
- b. External walls :  $U \leq 0.130 \text{ W/m}^2\text{K}$  Curtain walling (commercial)  $U \leq 1.20 \text{ W/m}^2\text{K}$
- c. Party walls :  $U = 0.0 \text{ W/m}^2\text{K}$  (solid party wall)
- e. Roof :  $U \leq 0.08 \text{ W/m}^2\text{K}$
- f. Window & frame :  $U \leq 0.80 \text{ W/m}^2\text{K}$ , Solar transmittance  $\leq 0.66$
- g. External (unglazed) door & frame :  $U \leq 1.0 \text{ W/m}^2\text{K}$
- h. Cold bridging :  $U \leq 0.07 \text{ W/m}^2\text{K}$  special construction joints applied..
- i. Thermal mass : TP250
- j. Ventilation : Humidity controlled natural ventilation + humidity controlled intermittent extracts or full MVHR.
- k. Air tightness : Design assumption  $\leq 3.0 \text{ m}^3/\text{m}^2\cdot\text{h}$
- l. Lighting. Low energy lighting. 100%
- m. Controls. 2 channel programmable time clock with Room thermostat and hot water thermostat.
- n. Circulation pumps. Class A variable speed pump
- o. Heating / hot-water system : Communal heating system with buffered hot-water (150L calorifier)
- p. Heating / hot-water source : City District Heating Network (CDHN)
- q. Renewable energy : CHP on CDHN or photovoltaic with communal mains gas boilers.
- r. Cooling system / source (commercial only) : Refrigerant cooling cop (EESER)  $\geq 3.8$
- s. Renewable energy : CDHN Combined heat & power with possible 5%-10% photovoltaic.



### Project overall energy data.

The overall energy and carbon dioxide data was calculated using the Deap and Neap energy calculation methodology and applying the Part L 2018 and 2017 for both the residential and commercial elements, Clongriffin district heating network energy calculations covered the following units and areas ;

Annual energy requirement	11,153,782	kWh/yr
24hr energy requirement (peak)	28,506	kWh/day
Primary energy equivalent	12,269,160	kWh/tr
24 hour peak demand winter	2790	kW
24 hour peak demand summer	1450	kW
24 hour peak demand average	1750	kW

Table 1

The project peak loads are accommodated by the thermal stores and peak load boiler array.

The CHP units are operated 24/7 at 100% capacity except for 1 week routine maintenance per year.

The overall project annual energy requirements (11,153,782 kWh) are met as follows :

Local peak block boilers	730,000	kWh/yr
CHP	9,551,391	kWh/yr
Central peak boiler array	899,360	kWh/yr
Total supplied	11,180,751	kWh/yr
Surplus	27,000	kWh/yr

Table 2

### CDHN / CHP energy and carbon dioxide savings.

The energy and carbon saving are measured against a conventional gas boiler system.

The energy savings achieved by the CHP plant are of particular use as they are also accepted as renewable energy under Part L.

#### Energy savings :

Equivalent energy for CHP thermal energy boiler @ 0.91	10,496,034	kWh/yr
Equivalent energy for CHP electrical energy CCGT @ 0.50	17,800,320	kWh/yr
Total equivalent energy for CHP	28,296,454	kWh/yr
Energy input for CHP	21,707,707	kWh/yr
Total energy saving (23.3%)	<b>6,688,647</b>	kWh/yr

#### Carbon savings :

Carbon emissions from CHP	4,406,665	kg/yr
Carbon emissions from local boilers	200,626	kg/yr
Carbon emissions from central boilers	162,846	kg/yr
Total system carbon emissions	4,770,137	kg/yr
Carbon emission avoided from electric generation	3,889,370	kg/yr
Nett carbon emissions from CDHN	880,767	kg/yr
Equivalent (gas boiler) carbon emissions	2,264,218	kg/yr
Total Carbon reduction (38.9%)	<b>-1,383,450</b>	kg/yr

### Renewable energy requirement and CDHN / CHP contribution.

Applying both the Deap and Neap with part L 2017 (commercial) and Part L 2018 (residential) energy the following are the renewable energy requirements and the provided renewable energy.

Renewable energy requirement residential	1,887,900	kWh/yr
Renewable energy requirement commercial	634,500	kWh/yr
Total Deap/Neap renewable energy requirement	2,522,400	kWh/yr
Renewable energy provided by CHP directive 2004/8/EC	<b>6,688,647</b>	kWh/yr
Renewable energy provided by CHP (Part L 2011 method)	<b>10,979,675</b>	kWh/yr

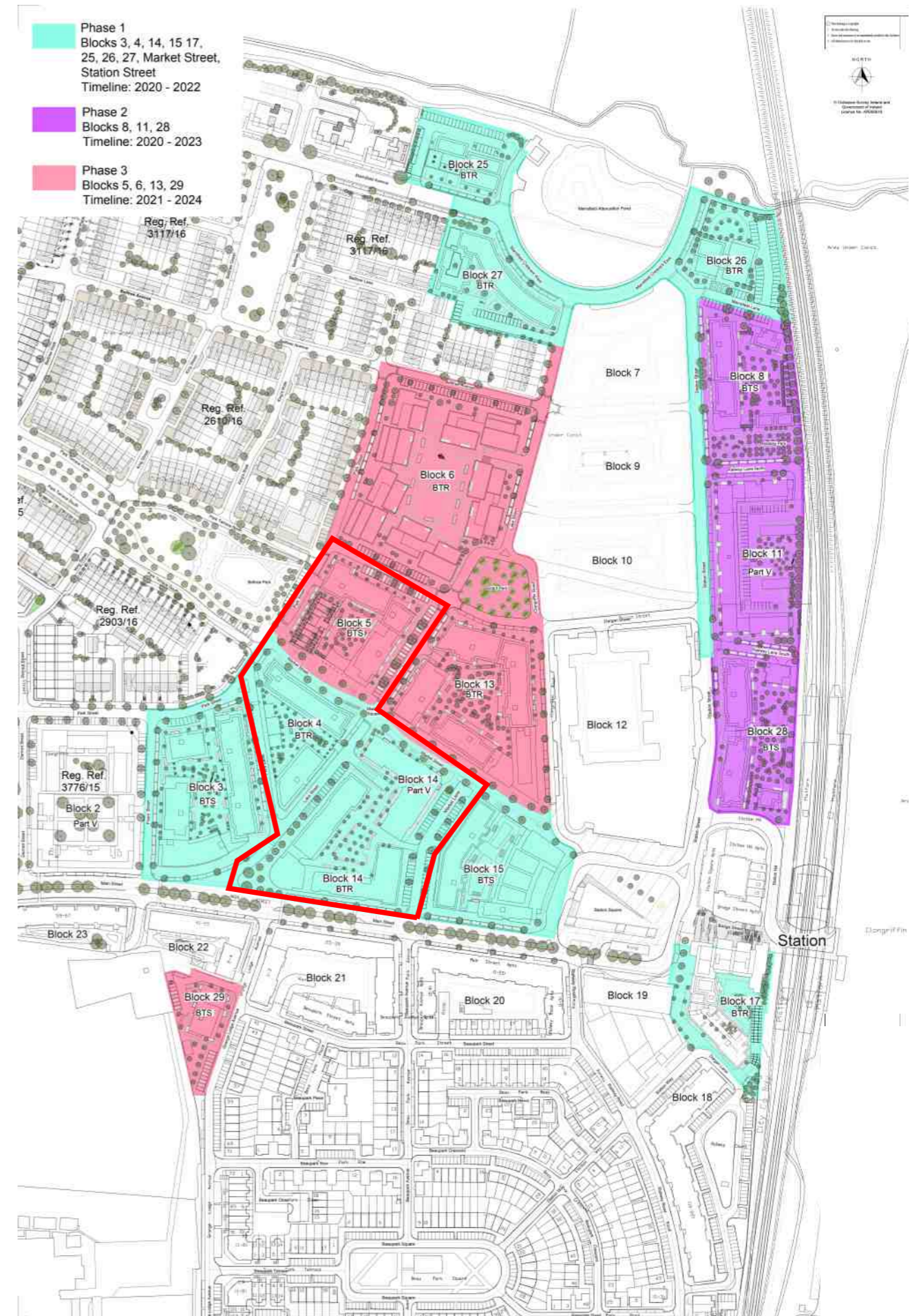


# 8.0 IMPLEMENTATION & PHASING

The three concurrent applications together represent a completion of the masterplan for Clongriffin. The blocks within the subject application fall into construction phases 1 and 3. If approved, construction of Phase 1 will commence in 2020, with later phases following as quickly after as feasible.

To date significant investment has occurred in Clongriffin with works and infrastructure in place to date including;

- Clongriffin Main Street, including the extension of the Malahide Road QBC up to the DART station
- Landscaping of principal streets and public spaces, including the main town square (Station Square) and high quality civic steps to the train station
- Re-development and enhancement of Fr. Collins Park (with assistance from special contributions from the completed development)
- Mayne River linear park – 350m completed up to the new attenuation pond and Linear Park between Fr. Collins Park and Station Square under construction
- A 483 space public Park and Ride facility beside the railway station and the QBC
- A 600+ public multi-storey car park to serve the new town
- Construction of the entire internal road network up to wearing course
- Construction of the entire internal drainage network (foul and surface water) with ongoing upgrades for SuDS as alterations are made to the permitted development by way of new planning applications;
- Completion and operation of the new DART station – this was entirely funded by Gannon Homes Ltd and Ballymore Homes (Helsingor), adjacent landowners in Fingal County Council
- 1,685 residential units and c.13,950sqm commercial and retail development completed.







Artist's view of Blocks 4 and 5 along Market Street



## 9.0 CONCLUSION



Since the first masterplan planning permission in 2003 Gerard Gannon Properties and Conroy Crowe Kelly Architects have taken a proactive role in working with both the local authority and local communities to develop a vibrant and thriving new town in Clongriffin.

As each phase of housing has come forward demand has only increased, with incoming residents recognising the exceptional opportunity to live in a sustainable and green, mixed-use neighbourhood, only 9.4km from the City Centre. Early phases have been extremely popular and Clongriffin is now home to an approximate population of 5,000.

More than half the planned homes and commercial spaces that were originally approved are either built, under construction or approved. This application, along with two concurrent applications, propose amendments to the outstanding blocks and a joined up proposal for completing the town.

This application is residential focussed, with 500 homes, 97 of which are Social Homes, 265 Build to Rent (BTR) and 138 Build to Sell (BTS).

Once this, and concurrent phases of Clongriffin are complete, the town will have the critical mass of residential density and commercial uses to support it's own, self-sustaining, local economy. Without undermining the links to the surrounding areas, it will be more than possible to live, be educated, shop and socialise within Clongriffin.





